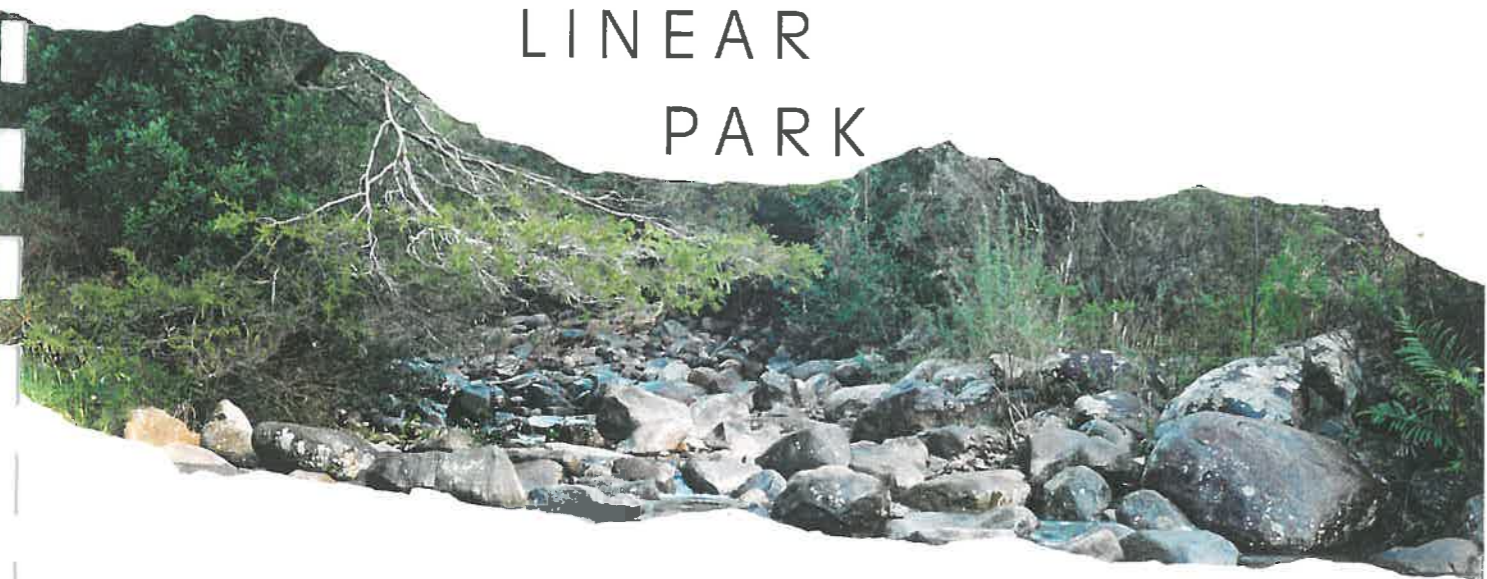
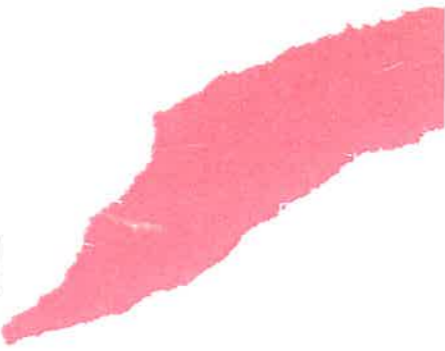


THE  
NEW TOWN  
RIVULET  
LINEAR  
PARK



A REPORT  
PREPARED  
FOR THE  
HOBART CITY  
COUNCIL BY:  
KATHARINA  
NIEBERLER,  
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1 APRIL 1995.



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## **Purpose**

This report is seen as an introduction to development of a Linear Park along the New Town Rivulet. The strategies suggested for its implementation are at planning level and the Draft Master Plan concentrates on the main objective of providing continuous access along the Rivulet.

The strategies suggested in the Site Inventory and Strategy Plan and the prepared Draft Master Plan are not fixed development plans but are seen rather as a starting point to a coordinated effort to achieve the set objectives for the establishment of the linear park. Regular review of the Master plan and refinement of strategies and design are anticipated in the future.

A closer link to issues such as siltation, water carrying capacity, water quality, flooding and suitable wildlife habitats are hoped for in the future.

The drawings have been chosen as the primary vehicle for dissemination of information as graphical representation of concepts and data can be assimilated much more readily than text. Hopefully this will provide the Councils with a practical document for day-to-day use by personnel involved in the long term implementation of the project.

The drawings consist of:

Site Inventory and Strategy Plan (total of 5 sheets)

Draft Master Plan (total of 5 sheets).

The master copy of all drawings (A1 format) can be viewed at the Council chambers. All drawing have been reduced for this report for ease of handling and are attached as Appendices.

## **Acknowledgments**

The project management was the responsibility of Mrs Regan T Douglas, Landscape Architect for the Hobart City Council. She supervised and organised the public launch of the project and also administered the public display in the Hobart area.

It was her foresight to see the potential of a linear park along the New Town Rivulet and in persisting and promoting the idea of a linear park enthused Glenorchy City Council to make it a joint effort to establish a Master Plan for the development and implementation of the linear park.

Mrs Kristine Ancher, Urban Design Coordinator with the Glenorchy City Council, organised meetings with Council staff and strongly supported and promoted the linear park idea. She also organised and supervised the public display at the Moonah Arts Centre.

Other participants were:

Hobart City Council:

Ian Hunter, Director – Parks, Community and Health Services  
Andrew Tompson – Director of Project Management  
Chee Liew – Director Environmental Services  
David Tan – Manager Engineering Design  
Leyon Parker – Director of Transportation Services  
Andrew Robert-Tissot – Technical Officer of Arboriculture  
John Rankin – Field Manager Reserves  
Neil Noye – Policy and Project Planner  
Peter Fleming – Field Surveyor

Glenorchy City Council

Andrew Lawrence – Manager Roads and Technical Services  
Sandra Hogue – Planning Officer  
Ray Thompson – Property Officer

Department of Environment and Land Management

Tracy Sparks – Tasmanian Property Group

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## Executive Summary

This report contains a brief assessment of the New Town Rivulet area for the purpose of establishing a Linear Park which stretches from the Derwent River to Mountain Park.

Following the site assessment a Strategy Plan was developed outlining the approach based on two main objectives.

The main objectives are:

To develop the New Town Rivulet area as a continuous linear park from the Derwent River to Mountain Park for the passive recreation and enjoyment of the community.

To protect the cultural and natural assets of the New Town Rivulet area.

The Strategy Plan suggests the following actions:

- Acquisition or the lease of land crucial to the implementation of the concept of a continuous linear park.
- Protection of natural features such as existing significant trees vegetation and rock formations.
- Protection of historic features and sites.
- Recommendations for the future land use along the rivulet.
- Green links to connecting open spaces.
- Functional links to existing and future community facilities.

The proposed Master Plan incorporates the identified strategies and provides guidance for the detailed design of the park. It also includes some entrepreneurial style development which may be considered for implementation.

The description of the Draft Master Plan includes:

- Access to the Park
- The main route through the Park
- Vegetation strategy
- Themes and interpretation
- Links to other public facilities
- Special park facilities
- Design considerations.

A brief description of the public forums conducted and issues raised in the process of this consultation has been included.

The report concludes with proposed Staging and Cost Planning, with a vision for the basic framework of the New Town Rivulet Linear Park being completed by the year 2006.

## **Introduction**

The idea to establish a Linear Park along the banks of the New Town Rivulet gained momentum in 1992 when as part of a Commonwealth employment program money became available to commence this challenging project.

The rivulet environ between Tower/Bowen Road and New Town Road was chosen for the implementation of Stage I. Design work was initiated, land leased from the Education Department (New Town High School), land purchased from residents in Mercer Street and permission sought from the Tasmanian Railway to allow attachment of a board walk to the railway bridge.

Towards the end of 1992 this first stage of the New Town Rivulet Linear Park was opened to the public. Construction for this first stage included a 130 m long timber board walk, a 600 m long gravel path and a dividing fence between the New Town High School and Linear Park premises. Weeds and unsafe trees were removed.

Following the implementation of Stage I the Hobart City Council, with support from the Glenorchy City Council, commissioned the preparation of a master plan for the New Town Rivulet Linear Park in 1994.

## **Site Assessment**

### History

Historically the New Town Rivulet area was developed in tandem with Sullivans Cove and was the prime area for the production of agricultural products, in particular dairy products, fruit and vegetables. As a result a wealth of constructed heritage can be discovered along the rivulet such as Lady Franklin Museum and other privately owned buildings such as Lauderdale or Pitt's Farm.

### Location

The New Town Rivulet stretches some 6.5 km from the Derwent River through industrial, suburban and semi-rural environments before it reaches Mountain Park on the lower foothills of Mount Wellington.

Mount Wellington can be seen from all parts of the New Town Rivulet area and acts as a visual reminder of the water's origin flowing downstream to the Derwent River.

Most of the rivulet area is within the boundaries of the Hobart City Council with the remainder being the responsibility of the Glenorchy City Council. Areas of crown land are currently maintained by the relevant council.

### The New Town Bay area

The New Town Bay area, characterised by its undeveloped open space and industrial development, will be the site for Hobart's sewage treatment plant upgrade. Hence the opportunity exists to improve the presentation of the water's edge and to incorporate a possible green link through to Cornelian Bay Cemetery and Cornelian Bay beyond.

The New Town Rivulet area is in close proximity to other recreational facilities, such as the netball courts on the corner of New Town and Creek Roads or John Turnbull Park in Lenah Valley. It is seen as a high priority to link these open spaces and provide safe and easy access to these public facilities.

### Vegetation

Exotic trees and weed species dominate the banks of the rivulet particularly the lower parts towards the Derwent River whereas the native vegetation becomes more dominant in the upper parts closer to Mountain Park. The most conspicuous and dominant environmental weed along the rivulet is the Crack Willow (*Salix x alba fragilis*). The crack willow spreads vegetatively by broken branches and twigs being carried downstream. It overgrows and shades out the native vegetation and by being deciduous may contribute to a change in the stream fauna. The crack willow also has a devastating effect on the water quality and quantity by transpiring enormous amounts of fresh water in summer when, in general, the water table is at its lowest. The Department of Primary Industry has released guidelines for the management of crack willows to counteract their devastating effect on streams, particularly apparent in the midlands of Tasmania.

The dominant tree cover, significant trees and significant vegetation have been identified and documented as part of the *Site Inventory*.

### The rivulet

Notwithstanding the detrimental effects of the crack willow a certain charm and ambience is conveyed by the gurgling water, the rocks, and the changing light and diaphanous screening proffered by the green vegetation in most sections of the rivulet.

The creek bed of the New Town Rivulet is formed in parts by beautiful rock formations, creating paddling pools and small waterfalls. These areas have a special charm and are of particular interest to children. One such rock formation is opposite Lady Franklin Museum.

Siltation, presumably caused amongst others by new housing development along the rivulet, is a serious environmental problem which increases the risk of flooding. The Hobart City Council, the Glenorchy City Council and the State Government have been negotiating regarding the amelioration of the siltation problem in New Town Bay. However no agreement has yet been reached.

In general the banks of the rivulet are difficult to access because of the rambling vegetation and private land holdings that either reach the

top of the bank or the centre line of the rivulet. The current land ownership (private, council or crown) along the rivulet has been indicated on the Site Inventory and Strategy Plan.

### Stage I

Works implemented during Stage I between Tower/Bowen Road and New Town Road have received praise from the community while, justifiably, at the same time receiving criticism for not providing easy access (disabled standard) at the entrance off New Town Road.

Easy access was a main point of focus during the design stage, however no physical solution was evident without compromising the privacy of adjacent land owners or without destroying the view of the historically significant New Town Road bridge. However, it is anticipated that the problem of easy access from New Town Road can be resolved in the long term.

This example highlights the difficulties in satisfying the conflicting interests of residents, park users and other interest groups while developing the linear park concept.

### **Strategy**

The *Strategy Plan* (Appendix A) outlines the approach which is based on two main objectives:

To develop the New Town Rivulet area as a continuous linear park from the Derwent River to Mountain Park for the passive recreation and enjoyment of the community.

To protect the cultural and natural assets of the New Town Rivulet area.

In order to achieve the main objectives a number of strategies have been developed. They are:

1. Acquisition or the lease of land crucial to the implementation of the concept of a continuous linear park.
2. Protection of natural features such as existing significant trees vegetation and rock formations.
3. Protection of historic features and sites.
4. Recommendations for the future land use along the rivulet.
5. Green links to connecting open spaces.
6. Functional links to existing and future community facilities.



### 1. Land to be acquired/leased

In order to achieve a continuous linear park along the New Town Rivulet it is necessary to acquire/lease parcels of land which are currently in private hands or are owned by the Crown. The *Site Inventory & Strategy Plan* shows which land is owned by the Crown, managed or owned by the City of Hobart and the City of Glenorchy.

The parcels of land to be acquired/leased have been given a priority ranking. The highest priority, priority one, identifies land which is crucial for the provision of continuous access along the rivulet. Negotiations to acquire these parcels of land should start as soon as possible.

Priority two identifies land which if acquired would greatly enhance the recreational and ecological value of the linear park. Land given a priority two ranking should be regarded as a long term consideration, however any opportunities to purchase these parcels of land should be taken up as soon as possible.

The Crown owns land along the banks of the rivulet, for example along Creek Road and on the corner of Albert and Risdon Roads. Land marked with L should be leased from the Crown, either by the Hobart City Council or the Glenorchy City Council, for the purpose of a permanent passive recreational facility, namely – *The New Town Rivulet Linear Park*. Negotiations should proceed as soon as possible.

Schedule 1 summarises the findings illustrated on the Site Inventory & Strategy Plan with regard to the acquisition or lease of land for the development of the Linear Park.

The actual size (width of land) to be acquired or leased needs to be considered carefully and may vary according to given site conditions. The height of the banks, the degree of slope, existing vegetation and their maintenance requirements, screening of industrial development and the provision for a pathway may influence the size of land that should be acquired or leased.

Location:

Reference:

**Priority One land – A1**

**Site Inventory & Strategy Plan – 5/5 (sheets 1 to 5)**

- |                                  |           |
|----------------------------------|-----------|
| • Pitt's Farm – near Albert Road | sheet 2/5 |
| • Lauderdale, 74 Risdon Road     | sheet 2/5 |
| • Tasmaid, Lenah Valley Road     | sheet 4/5 |

**Priority Two land – A2/AS**

- |   |           |
|---|-----------|
| • 123 Albert Road   | sheet 2/5 |
| • 125 Albert Road   | sheet 2/5 |
| • Mercury Walsh, 1–3 Bowen Road                                     | sheet 2/5 |
| • Southern Star, 5 Bowen Road                                       | sheet 2/5 |
| • Vacant land along Bowen Road owned by the Retirement Benefit Fund | sheet 2/5 |

- Land owned by the Titan Stanley Factories sheet 2/5
- New Farm Greenleas sheet 3/5
- Properties off Hamel Street bordering the rivulet sheet 3/5
- Leura, occupied by car rental sheet 3/5
- 10 Main Street, owned by GCC sheet 3/5
- Ray Taylor's vacant land opposite 103 Creek Road sheet 3/5
- 60 Creek Road sheet 3/5
- Annie Cooper's, 48 Creek Road sheet 4/5
- Market Garden opposite John Turnbull Park sheet 4/5
- 221 Lenah Valley Road sheet 4/5
- 227 Lenah Valley Road sheet 4/5
- ? Lenah Valley Road sheet 5/5
- 400 Lenah Valley Road sheet 5/5
- 464 Lenah Valley Road sheet 5/5

**Land to be leased from the Crown – L**

- Crown Reserve between the Rivulet and Albert Road sheet 2/5
- Crown land between the rivulet and Wilmslow Avenue sheet 2/5
- New Town High school, small parcel off Bowen Road sheet 2/5
- Crown Land between the rivulet and Creek Road sheet 3/5
- Crown Land between the rivulet and Creek Road sheet 4/5

*Table 1 Acquisition or lease of land recommended for the development of the New Town Rivulet Linear Park.*

The control of both sides of the New Town Rivulet is important for the effective management and maintenance of the park. This applies to both major functions of the rivulet area which are providing passive recreation opportunities and effective water discharge. Control of both banks of the rivulet will also enable better weed management and protection of existing valuable vegetation as well as offering the opportunity to establish smaller loop tracks in the future.

In addition, investigations into the rights and land holdings of the Marine Board in New Town Bay should also be considered.

The list for land acquisition/lease should not be regarded as conclusive, but as a starting point for discussions with an intent towards a coordinated effort to acquire land in order to achieve the goal of establishing a linear park.

**2. Natural features to be protected**

Natural features such as existing significant trees, native plant communities further upstream towards Mountain Park and the

identified sites of rock formation warrant protection. Some of the identified significant trees are relics of the past and an insight to the former indigenous native vegetation that covered the banks of the rivulet prior to white settlement.

The opportunity also exists to use the Linear Park to establish a wildlife corridor for native flora and fauna ranging from Mt Wellington, with its rich resource of native plant and wildlife, to the banks of the Derwent River. The re-introduction of native vegetation, particularly in the lower parts of the rivulet would be a sound ecological decision, with the benefit of creating a unique character for the New Town Rivulet Linear Park.

### **3. Historic features to be protected**

The number of historic features and sites along the New Town Rivulet have been identified and described by historian Lindy Scripps in her report *The New Town Rivulet Historical Study. A report for the Parks and Recreation Department, City of Hobart, 1993.*

Identified significant buildings and structures such as Pitt's Farm or the New Town bridge are an integral part of the feel and experience of the New Town Rivulet area. They contribute to the special characteristic and uniqueness of the linear park by adding interest and visual enjoyment while walking through the park.

Also the interpretation of the natural and built environment should form part of the long term development strategy. Interpretation may be seen as a preventative measure against vandalism (the more people know about their environment the more they value it).

### **4. Recommended land use**

Part of the *Strategy Plan* is also the further development of housing and the consolidation of existing housing along the New Town Rivulet. Re-zoning of land from industrial to residential should be considered seriously on the Glenorchy side of the New Town Rivulet area.

Also higher density housing should be considered.

For all new subdivisions a minimum of a 5% subdivision contribution should be taken along the banks of the Rivulet as well as easements to improve the access to the linear park.

This 5% subdivision contribution should be regarded as a minimum with a provision to increase it if an enlarged contribution would benefit the function of the park in that particular area. Therefore each subdivision application should be treated on its own merits.

Sites to be acquired as part of the subdivision contribution have been marked on the drawings with AS.

Care should be taken to ensure that new development does not negatively affect the safety or quality of the open space experienced by

the park user. For example no further private access should be granted through the linear park.

#### 5. Green Links

A desirable long term goal is the connection of the New Town Rivulet Linear Park with other existing open space to create a network of green spaces offering an alternative route as a preference to footpaths along busy roads or simply for the purpose of "walking for pleasure".

Proposed green links are seen as a long term proposition, however provision should be made now as these green links are crucial to the implementation of the New Town Rivulet Linear Park and the notion of a green network through the suburban environment.

Suggested are green links connecting the New Town Rivulet Linear Park with:

Cornelian Bay Cemetery and Cornelian Bay beyond.

New Town High School (existing in principle).

the major Glenorchy Council Reserve off Creek Road and Springfield Avenue beyond.

New Town Primary School and Ogilvie High School.

John Turnbull Park, Haldane Reserve and Mount Stuart to the south and Springfield avenue to the north.

Lady Franklin Museum, Tasmanian Native Flora Garden and Brushy Creek Linear Park (at this stage proposed only).

Kalang Avenue.

Mountain Park.

#### 6. Functional Links

Functional links have been indicated where the continuation of the linear park involves a major road crossing, such as the crossing of the Brooker Highway or where a functional link to existing community facilities is desirable, for example, the link of the existing board walk with the Glenorchy to Hobart cycle way. The identification of functional links is important for the location of pathways in the linear park and for access to the linear park in general.

The treatment of these functional links, with nearly all of them including a road crossing, should be further investigated with input from a traffic engineer in order to achieve the best results for pedestrian and vehicular road safety.

The control of vehicular traffic adjacent and in some instances through the park is closely tied to pedestrian safety and the quality of ambience

experienced by the park user. Some traffic control measures (walk through traffic islands) have been indicated in Creek Road. These traffic islands are a suggestion only and are intended as a starting point for further discussions.

Functional links are indicated on the drawings :

at the crossing of Brooker Highway, Risdon and Bowen Road.

at the crossing of Albert Road to reach Talire School.

to allow access to New Town High School (existing in principle).

to connect to the Glenorchy to Hobart Cycle Way.

at Creek Road to reach the netball courts and St Johns Park.

at the road crossing with Gerrard Street.

at the road crossings with Lenah Valley Road in various places.

The number of above functional links may not be complete and further desirable links may come to light with the development and implementation of the linear park concept.

### **The Draft Master Plan**

The purpose of the *draft Master Plan* (Appendix B) is seen as being the blue print for the future development of the New Town Rivulet Linear Park.

#### *The Draft Master Plan*

incorporates the information and strategies identified on the *Site Inventory and Strategy Plan*. **Note:** Land recommended for purchase or lease in the *Strategy Plan* has been shown implemented on the *draft Master Plan* to provide a more complete picture.

provides guidance for the detailed design of park facilities and the scope of work that can be expected.

suggests some entrepreneurial style development which might be considered (for example the proposed redevelopment of the New Town Bay area).

The written description of the *draft Master Plan* has been divided into seven sections:

1. Access to the Park
2. The main route through the Park
3. Vegetation strategy
4. Themes and interpretation
5. Links to other public facilities

6. Special park facilities

7. Design considerations.

**1. Access to the Park**

The front door environment and treatment of the entries (main access points to the park) are important for the recognition of the park as a major community facility, in the same way as the facade and front door of a building indicates the owner's rank and status. Every entry design to the park should be considered carefully to achieve the desired result, suitable for the particular site and environment.

Main access points to the park have been identified at:

New Town Bay

at the intersection with Albert and Risdon Road

at the intersection with Bowen Road

at the intersection with New Town Road

at the intersection with Lenah Valley Road (entry I)

at the intersection with Lenah Valley Road, near Rangeview Crescent

at the intersection with Lenah Valley Road (entry II)

at the intersection with Mountain Park.

As indicated on the drawings every park entry should have a "sense of entry" and appropriate signage to inform and direct the park user. Planting and art work (eg. totems) are intended as design tools to make the entries instantaneously recognisable. If appropriate, seating at major entry points may also be considered.

Refer to Appendix E for the landscape design of Tower/Bowen Road and New Town Road park entries.

Also the naming of major entry points is conducive to the idea of the linear park development. Naming should be consistent and innovative. Public involvement in the naming of park entries is desirable. One suggestion is to base the naming of the park entries on the aboriginal language of the people that lived in the area before and during white settlement.

**2. The main route through the park**

The objective of providing a continuous link and hence continuous access along the banks of the New Town Rivulet have been shown on the *draft Master Plan* as a main route through the park.

The alignment of this main route will be crucial to the feel the park user will experience while walking in the park. Detailed design

considerations with regard to the path alignment are considered essential before the implementation of each section. The location of pathways shown on the drawings may have to be reassessed during the design and design development stage for each section.

The average width of the pathway should be approximately 2 m in the lower section of the park and approximately 1.2 m in the upstream section of the park starting from Lenah Valley Road park entry II.

Red Gravel is seen as an appropriate path surface and more elaborate path edging may be used in the lower parts of the park as part of the entry design. Path grades to disabled standards (1:14) are anticipated for most sections of the park. Raised timber board walks similar to the design used in Stage I is anticipated for steep and difficult sections of the Linear Park.

Bridging of the rivulet is indicated in various locations on the drawing and each bridge design should be considered separately relating to the characteristics or specialised use of the area. The proposed bridge near Lady Franklin Museum for example could have Japanese connotations relating to the proposed Yaizu No Niwa project nearby. Bridging of the rivulet in the upper parts of the park (upstream from Lenah Valley Road entry II) should be more basic, reflecting the close proximity to Mountain Park. Each bridge design should take into consideration required maintenance of bridge elements.

At this stage a shared cycle/ pedestrian use of the main route through the park is anticipated.

### 3. Vegetation strategy

The New Town Rivulet Linear Park is seen as an opportunity to establish a wildlife corridor for native flora and fauna between the Derwent River and Mountain Park. In the upper one third of the park native vegetation is dominant with weed species confined to the understorey. The lower part of the park is dominated by crack willows and other weedy understorey species. The re-introduction of native vegetation requires a long term program of weed eradication, which should start in the least infested areas (upper parts of the rivulet) and continue downstream, to lessen the impact of weeds spreading.

Plants species listed in Table 2 are recommended for revegetation purposes and represent the plant community that can be assumed was once indigenous to the area.

Acacia dealbata	Acacia mearnsii
Acacia melanoxylon	Acacia verticillata
Agrostis aemula	Allocasuarina littoralis
Astroloma humifusum	Banksia marginata
Bauera rubioides	Bursaria spinosa
Callistemon pallidus	Carex appressa
Carex tasmanica	Danthonia caespitosa
Danthonia pilosa	Danthonia sp
Deyeuxia quadriseta	Dianella revoluta
Dianella tasmanica	Dichelachne sp

Dodonea viscosa	Eucalyptus amygdalina
Eucalyptus viminalis	Exocarpos cupressiformis
Leptospermum lanigerum	Lomandra longifolia
Microlaena stipoides	Poa labillardieri
Poa sp	Schoenus apogon
Stipa sp	Themeda australis

Table 2 Plant species recommended for re-establishment

The re-establishment of indigenous plant life along the banks of the rivulet will have ecological benefits and it is anticipated that a variety of animals, in particular birds can be attracted to the park environment. The re-establishment of indigenous plant communities is a long term process which should include and be supported by the community.

Deirdre J. Johnson's map *Vegetation of the Hobart Municipality, 1994* provides valuable information on the composition of native vegetation in particular further upstream towards Mountain Park.

#### 4. Themes and interpretation

One consistent theme throughout the park may be the native vegetation (also refer to 3. Vegetation Strategy), other themes could include the naming of park entries (also refer to 1. Access to the Park) and the display of art work throughout the park. The display of timber totems with carved wildlife on the top (different animals featuring at different park entries) is seen as an appropriate start to the display of art and sculptures throughout the Park.

A heritage theme is anticipated for interpretation in the lower parts of the Park, whereas interpretation themes in the upper parts should be based on the natural environment. The display of interpretation and art in the Park should be consistent, however unobtrusive, conveying a sense of discovery. Linked to the interpretation should be a standard layout and design of directional signs for the Park.

#### 5. Links to other public facilities

The detailed design of green and functional links need to be considered with regard to safety, if a road crossing is involved, and with regard to access and user convenience. Principles of appropriate traffic control and pedestrian safety are to be established in consultation with a traffic engineer.

Bottle neck treatment (narrowing of the road), walk through traffic islands, the installation of lights and the use of bollards may all be appropriate means of control, however a traffic management strategy for the whole Park is recommended

#### 6. Special park facilities

Special park facilities include suggested treatment of the New Town Bay area, the proposed Wilmslow Avenue Rivulet Park & Sculpture Playground, the upgrading of John Turnbull Park, the implementation



of the "Yaizu No Niwa" project and improvements to the barbecue shelter area on the boundary to Mountain Park.

#### The New Town Bay area

Suggested improvements to the New Town Bay area include a new boat ramp and public jetty, a pedestrian bridge across the rivulet, upgrading of the rowing club facilities to include public amenities ie toilets, a cafe, children's play area, canoe hire and so forth. The improvements suggested by the consultant have been developed in consultation with council staff and by incorporating comments from residents.

The proposed redevelopment, together with possible development and investment from the private sector should be given serious consideration.

#### Wilmslow Avenue – Rivulet Park & Sculpture Playground

This project was designed and documented for construction in 1993 as an Australian Council–Community Arts Project. The implementation of the Wilmslow Avenue project would be a suitable extension and additional attraction for the New Town Rivulet Linear Park.

The complete set of drawings for the Wilmslow Avenue – Rivulet Park & Sculpture Playground is attached as Appendix D.

#### John Turnbull Park

John Turnbull Park is of considerable size, however under–developed and under–utilised. It is suggested that as part of the New Town Rivulet Linear Park development the role and facilities of John Turnbull Park be re–assessed.

The green link from the New Town Rivulet through John Turnbull Park to Haldane Reserve and Mount Stuart beyond is seen as a major step towards the realisation of the green link network.

#### Yaizu no Niwa

The Yaizu no Niwa project was proposed in the late 1980's as part of a public housing development. It includes paths, a lake with island, pavilions and planting. Planting for this considerable size park development is proposed in Japanese style using Australian native plants. Adjacent revegetation works is proposed with indigenous flora. The implementation of the Yaizu no Niwa park will no doubt enhance the qualities of the New Town Rivulet Linear Park project.

A copy of the concept design for the Yaizu no Niwa park development has been attached as Appendix C.

### Barbecue shelter area on the boundary to Mountain Park

The Hobart City Council is anticipating the upgrading of the barbecue shelter area on the boundary to Mountain Park in the near future. Improved facilities include upgrading of the shelter and the provision of toilets and planting to increase the amenity of the area.

### **7. Design considerations**

The section of the Rivulet between New Town Bay and the Lenah Valley Park Entry II is characterised by industrial development, housing and a sub-urban environment.

The section of the Rivulet between the Lenah Valley Park Entry II and Mountain Park is characterised by rural properties set amongst native forests and woodland which extend like fingers from the splendid natural vegetation of Mount Wellington.

These characteristics should be reflected in the treatment and detail design of the New Town Rivulet Linear Park.

The section from New Town Bay to Lenah Valley Road Park entry II is to reflect the more urban style park development by having an average path width of 2 m, sophisticated timber board walks, interpretation of the cultural heritage and, in the long term, appropriate lighting to allow the use of the linear park at night.

The section from Lenah Valley Road Park entry II to Mountain Park is seen as a transition from urban to minimal impact style park development with narrower (1.2 m wide) gravel or mulched tracks, basic creek crossings and interpretation that concentrates on the natural heritage.

### **Public Consultation**

Mrs Regan T. Douglas contacted a number of residents personally and visited their homes and work places to discuss the Linear Park and the resulting possible impact on residents living along the rivulet.

On 12 January 1995 a detailed presentation of the Draft Master Plan for the New Town Rivulet Linear Park was held by Ms Regan Douglas at the Lenah Valley Community Hall. A summary of issues discussed at the public meeting is attached as Appendix F.

Following the public meeting a display of the Draft Master Plan Drawings was arranged in the town hall of the Hobart City Council and at the Moonah Arts Centre.

In brief the public's response to the Linear Park Draft Master Plan has been positive and encouraging.

Some residents were concerned about the weeds along the rivulet, vandalism in the area, the water quality of the rivulet, the threat of

flooding and further siltation. Other issues raised included the provision of car parking, public toilets and barbecue facilities.

Also the questions of proposed time frame and costs involved in the implementation of the Linear Park were raised.

A public meeting before the implementation of each Stage has also been requested.

As a result of the public consultation process following adjustments have been made to the Draft Master Plan. They include:

connection of the Linear Park to the Wilmslow Avenue – Rivulet Park and Sculpture Garden.

alterations to the location of the main route near the intersection of Alwyn and Lenah Valley Road.

provision for a green link to the proposed Brushy Creek Linear Park.

review of the main track route between Lenah Valley Park Entry II and Mountain Park.

These adjustments have been marked on the Draft Master Plan drawings as amendments February 1995.

### **Staging and Cost Planning**

The 6.5 km long New Town Rivulet Linear Park has been divided into seven sections for staging and cost planning purposes.

The seven sections indicate easily recognisable geographical boundaries shown on the *Draft Master Plan*. Some sections have been further divided into sub-sections indicating different stages within each section.

The stage numbering, starting with the first section already implemented, refers to the relevant section (I to VII) and sub-section (I to I A, II to II A).

The proposed *Draft Master Plan* is seen as the first realisation to the long term implementation of a linear park along the New Town Rivulet. Priority has been given to the implementation of continuous access along the banks of the rivulet with other anticipated improvement to the park to occur over time.

Therefore the staging and cost planning will be limited to the implementation of the continuous access in the park rather than providing a figure for the implementation of all features shown on the *Draft Master Plan* drawings.

The costs involved in the proposed acquisition and leasing of land have, because of their complexity and varying nature, not been included in the costing. The order of cost given for each stage is

indicative and for budgetary considerations only and has been derived from estimations based on the construction costs of Stage I. The order of cost for each stage is based on present day value and includes three components essential for the provision of a continuous access along the rivulet.

The order of cost\* for each stage includes an allowance for:

**the construction of park entries contained in each section .**  
This may include fencing, paving, planting and seating.

**the construction of the main route.**  
This may include a gravel path, timber board walk, bridging, fencing and planting.

**the provision of sundry minor additions.**  
This may include art work, temporary signage and improvement to existing structures.

The proposed staging anticipates the implementation of all sections by the year 2006. Implementation periods have been worked out in consultation with representatives from the Hobart City Council and Glenorchy City Council.

Details of the Staging, including order of cost, implementation period and responsibilities are shown in Table 3.

The relationship of implementation period and order of cost per stage have been graphically indicated in Table 4.

The order of cost for the implementation of all stages is \$1.18 Million, with contributions from the Hobart City Council of \$900 000.00 and from the Glenorchy City Council of \$280 000.00.

Table 5 provides details of the contributions by the respective Council.

\*

The order of cost does not include:

- costs incurred in the acquisition or lease of land,
- future maintenance costs,
- council overheads and professional fees for design and contract administration,
- external lighting,
- alterations to existing services
- surveying fees,
- special park facilities, such as the implementation of the Wilmslow Avenue project
- or the redevelopment of the New Town Bay area,
- weed management and the long term implementation of the revegetation strategy.
- traffic control measures.

Section <sup>1</sup>	Sub-Section <sup>2</sup>	Stage <sup>3</sup>	Order of Cost in \$ <sup>4</sup>	Implementation <sup>5</sup>	Responsibility <sup>6</sup>
New Town Bay to Risdon Road		VII	120 000.00	2005/2006	HCC
Risdon Road to Bowen Road		III	110 000.00	1999/2000	HCC/GCC
Bowen Road to New Town Road	Bowen Rd to <sup>7</sup> New Town Rd	I	130 000.00 <sup>8</sup>	1992/1993	HCC
	Bowen & New Town Road Park Entries Connection to Cycle Way	I A	30 000.00	1995/1996	
New Town Road to John Turnbull Park	Rivulet realignment & <sup>9</sup> Creek Rd stabilisation	II	68 000.00	1994/1995	GCC
	New Town Rd to John Turnbull Park	II A	180 000.00	1996/1998	
John Turnbull Park to Lenah Valley Road Rangeview Cres. Park Entry		IV	215 000.00	1997/1998	HCC
Lenah Valley Road/Rangeview Cres. Park Entry to Lenah Valley Road Park Entry II		V	170 000.00	2001/2003	HCC
Lenah Valley Road Park Entry II to Mountain Park		VI	160 000.00	2003/2004	HCC

1 The seven sections indicate easy recognisable geographical boundaries shown on the Draft Master Plan.

2 Sub-Sections indicate different stages within each section.

3 Stage numbering refers to relevant section (I to VII) and subsection (I to I.A, II to II.A).

4 Order of cost is indicative and for budgetary considerations only and includes the construction of park entries, construction of the main route and provisions for sundry minor additions.

5 Implementation refers to the period of anticipated implementation for each stage.

6 Responsibility refers to the responsible council for each stage. HCC = Hobart City Council, GCC = Glenorchy City Council

7 Stage I was implemented in 1992/1993.

8 Represents construction costs for stage I.

9 Stage II is currently in the design development phase.

Table 3 Staging of the New Town Rivulet Linear Park including order of cost, implementation period and responsibilities

	1992	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002	2003	2004	2005	2006
Stage I <sup>1</sup>	##### (\$130 000.00)														
Stage I A <sup>1</sup>	##### (\$30 000.00)														
Stage II <sup>2</sup>	##### (\$68 000.00)														
Stage II A <sup>2</sup>	##### (\$180 000.00)														
Stage III <sup>3</sup>	##### (\$110 000.00)														
Stage IV <sup>1</sup>	##### (\$215 000.00)														
Stage V <sup>1</sup>	##### (\$170 000.00)														
Stage VI <sup>1</sup>	##### (\$160-000.00)														
Stage VII <sup>1</sup>	##### (\$120 000.00)														
Total HCC	1992	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002	2003	2004	2005	2006
Total GCC	-\$130 000.00-	-\$30 000.00-	-\$30 000.00-	-\$30 000.00-	-\$215 000.00-	-\$180 000.00-	-\$77 000.00-	-\$77 000.00-	-\$77 000.00-	-\$170 000.00-	-\$170 000.00-	-\$160 000.00-	-\$160 000.00-	-\$120 000.00-	-\$120 000.00-

1 Responsibility of the Hobart City Council  
 2 Responsibility of the Glenorchy City Council  
 3 Joint responsibility (HCC 70% - GCC 30%)

Table 4 Implementation Period and Order of Cost

Contribution from the Hobart City Council:

Stage	Order of Cost	Implementation Period
I	\$130 000.00	1992/1993
I A	\$ 30 000.00	1995/1996
IV	\$215 000.00	1997/1998
III	\$ 77 000.00 <sup>1</sup>	1999/2000
V	\$170 000.00	2001/2002
VI	\$160 000.00	2003/2004
VII	\$120 000.00	2005/2006
=====		
		\$900 000.00 = \$ 65 000.00/Year

1. Hobart City Council's contribution of 70% to Stage III

Contribution from the Glenorchy City Council:

Stage	Order of Cost	Implementation Period
II	\$ 68 000.00	1994/1995
II A	\$180 000.00	1996/1998
III	\$ 33 000.00 <sup>2</sup>	1999/2000
=====		
		\$281 000.00 = \$ 47 000.00/Year

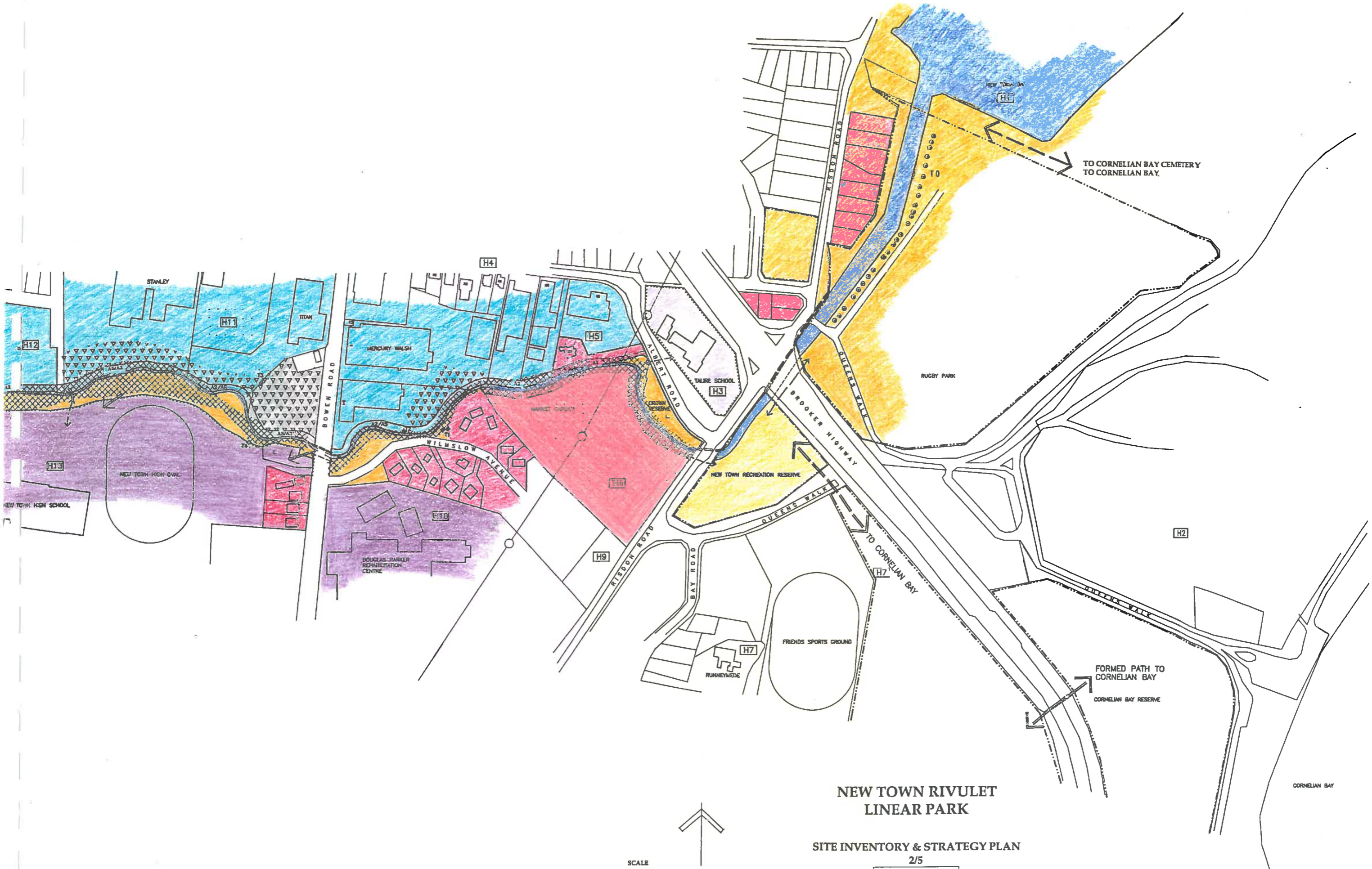
2. Glenorchy City Council's contribution of 30% to Stage III

*Table 5 Contribution – Hobart City Council & Glenorchy City Council*

## **Appendix A**

### **New Town Rivulet Linear Park Site Inventory & Strategy Plan**





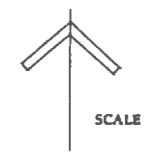
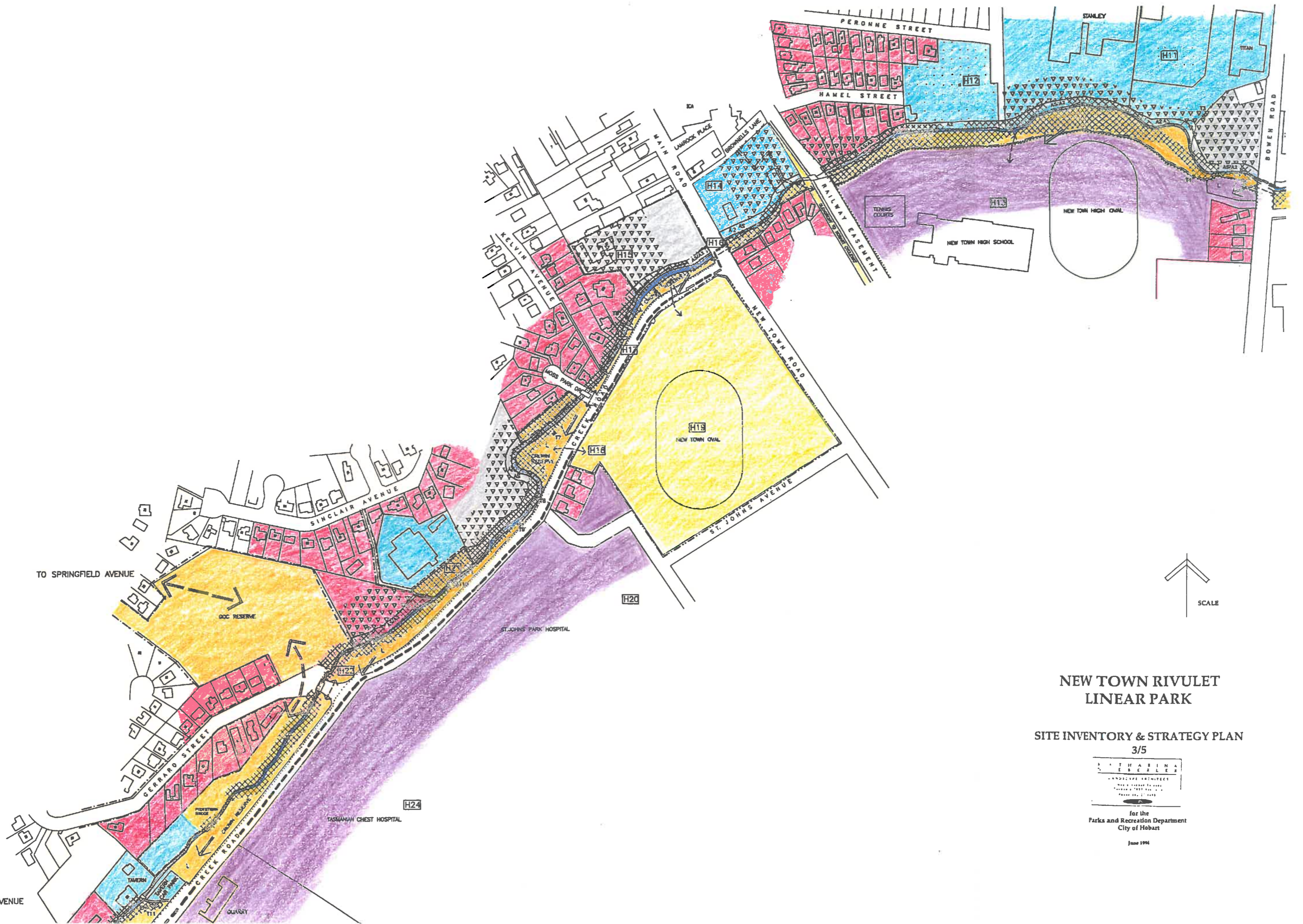
### NEW TOWN RIVULET LINEAR PARK

SITE INVENTORY & STRATEGY PLAN  
2/5

KATIA ABINA  
NIEBERGELER  
LANDSCAPE ARCHITECTS  
3 South Street, Hobart  
Tasmania 7502 Australia  
Phone 002 51 0010

for the  
Parks and Recreation Department  
City of Hobart

June 1994



## NEW TOWN RIVULET LINEAR PARK

SITE INVENTORY & STRATEGY PLAN  
3/5

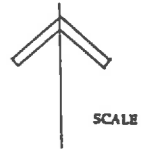
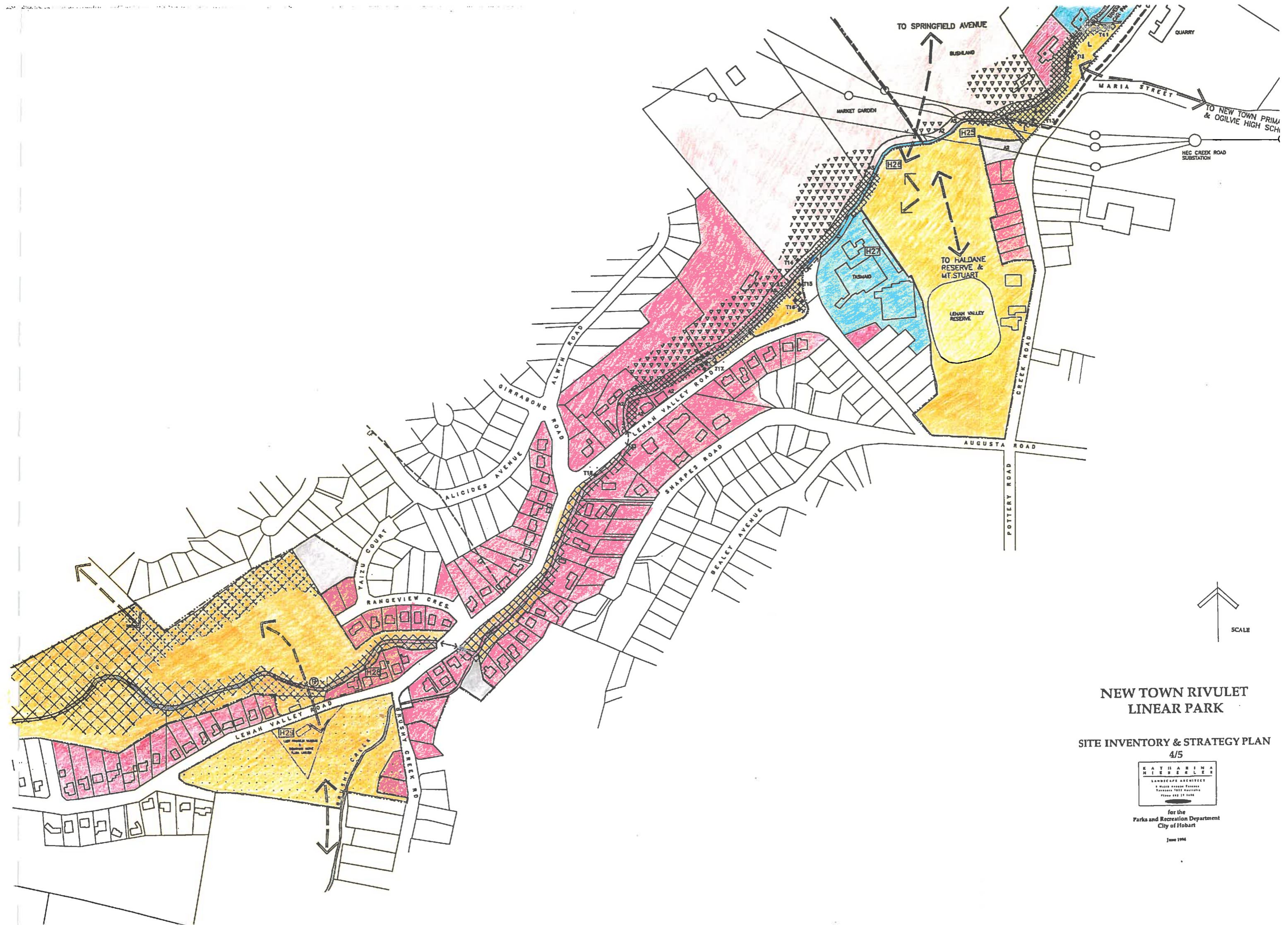


for the  
Parks and Recreation Department  
City of Hobart

June 1994

TO SPRINGFIELD AVENUE





**NEW TOWN RIVULET  
LINEAR PARK**

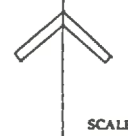
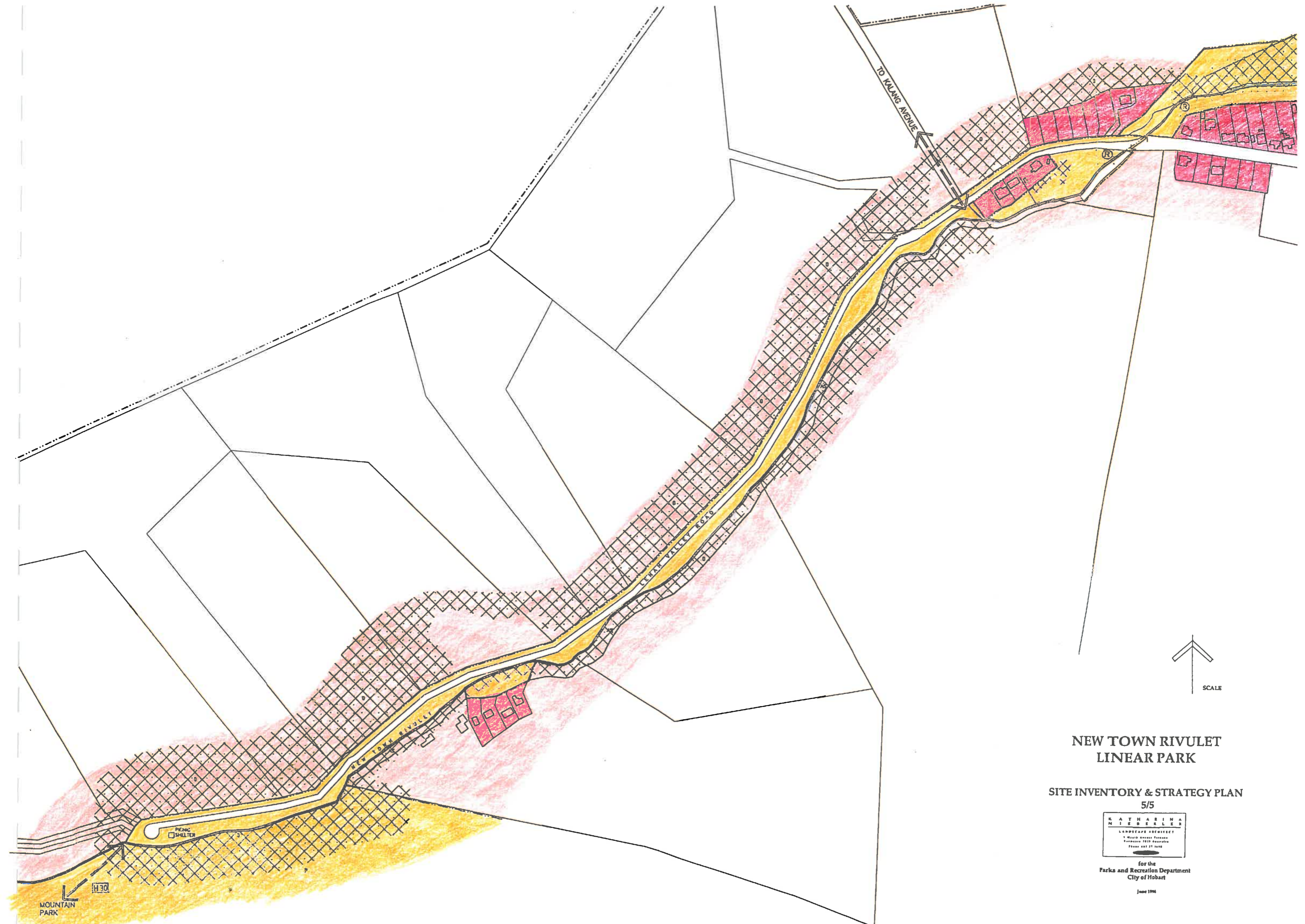
**SITE INVENTORY & STRATEGY PLAN**

4/5



for the  
Parks and Recreation Department  
City of Hobart

June 1994



# NEW TOWN RIVULET LINEAR PARK

SITE INVENTORY & STRATEGY PLAN  
5/5



for the  
Parks and Recreation Department  
City of Hobart

June 1996

**Appendix B**

**New Town Rivulet Linear Park  
Draft Master Plan**



..... along Creek Road.

..... opposite Lady Franklin Museum.

## NEW TOWN RIVULET LINEAR PARK

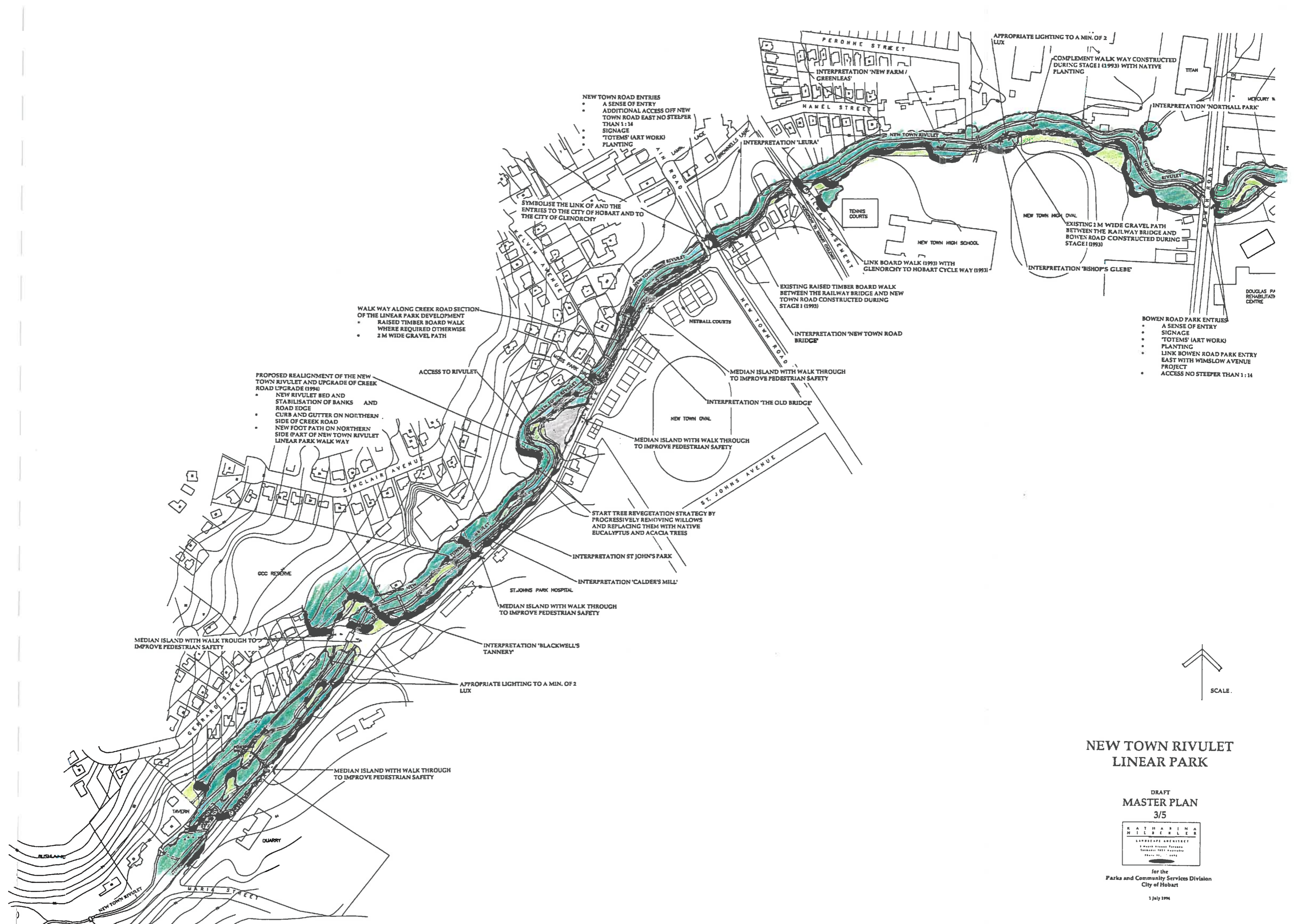
DRAFT  
MASTER PLAN  
1/5



for the  
Parks and Community Services Division  
City of Hobart

1 July 1994





- NEW TOWN ROAD ENTRIES**
- A SENSE OF ENTRY
  - ADDITIONAL ACCESS OFF NEW TOWN ROAD EAST NO STEEPER THAN 1:14
  - SIGNAGE
  - TOTEMS (ART WORK) PLANTING

SYMBOLISE THE LINK OF AND THE ENTRIES TO THE CITY OF HOBART AND TO THE CITY OF GLENORCHY

- WALK WAY ALONG CREEK ROAD SECTION OF THE LINEAR PARK DEVELOPMENT**
- RAISED TIMBER BOARD WALK WHERE REQUIRED OTHERWISE
  - 2 M WIDE GRAVEL PATH

- PROPOSED REALIGNMENT OF THE NEW TOWN RIVULET AND UPGRADE OF CREEK ROAD UPGRADE (1994)**
- NEW RIVULET BED AND STABILISATION OF BANKS AND ROAD EDGE
  - CURB AND GUTTER ON NORTHERN SIDE OF CREEK ROAD
  - NEW FOOT PATH ON NORTHERN SIDE (PART OF NEW TOWN RIVULET LINEAR PARK WALK WAY)

START TREE REVEGETATION STRATEGY BY PROGRESSIVELY REMOVING WILLOWS AND REPLACING THEM WITH NATIVE EUCALYPTUS AND ACACIA TREES

- BOWEN ROAD PARK ENTRIES**
- A SENSE OF ENTRY
  - SIGNAGE
  - TOTEMS (ART WORK) PLANTING
  - LINK BOWEN ROAD PARK ENTRY EAST WITH WIMSLOW AVENUE PROJECT
  - ACCESS NO STEEPER THAN 1:14

# NEW TOWN RIVULET LINEAR PARK

DRAFT  
MASTER PLAN  
3/5



for the  
Parks and Community Services Division  
City of Hobart

1 July 1996



APPROPRIATE LIGHTING TO A MIN. OF 2 LUX

LINK TO IMPROVED ACTIVE AND PASSIVE RECREATION FACILITIES IN JOHN TURNBULL PARK AND TO HALDANE RESERVE BEYOND

INTERPRETATION ORPHAN SCHOOL WATER SUPPLY

RAISED TIMBER BOARD WALK ALONG THE NORTHERN BOUNDARY OF TASMAID

CREEK ROAD / JOHN TURNBULL PARK ENTRY  
 • A SENSE OF ENTRY  
 • ACCESS NO STEEPER THAN 1:14  
 • SIGNAGE  
 • 'TOTEMS'  
 • PLANTING

INTERPRETATION 'BAKER'S MILK'

'BOTTLE NECK TO SLOW TRAFFIC AND TO IMPROVE PEDESTRIAN SAFETY'

IMPLEMENTATION OF YAIZU NO NIWA PROJECT (JAPANESE STYLE PARK WITH TASMANIAN NATIVE PLANTS)

LINK TO YAIZU COURT

'BOTTLE NECK TO SLOW TRAFFIC AND TO IMPROVE PEDESTRIAN SAFETY'

LENAH VALLEY ROAD PARK ENTRIES  
 • A SENSE OF ENTRY  
 • SIGNAGE  
 • 'TOTEMS'  
 • PLANTING

Amendment February 1995  
 CONTINUE WALK WAY ALONG LENAH VALLEY ROAD, INCLUDE ROAD CROSSING IN NEW INTERSECTION DESIGN OF ALWYN AND LENAH VALLEY ROAD

LINK TO KALANG AVENUE

APPROPRIATE LIGHTING TO A MIN. OF 2 LUX

LENAH VALLEY ROAD/RANGEVIEW CRES. PARK ENTRIES  
 • A SENSE OF ENTRY  
 • SIGNAGE  
 • 'TOTEMS' (ART WORK)  
 • PLANTING

INTERPRETATION 'LADY FRANKLIN MUSEUM & TAS. NATIVE FLORA GARDEN'

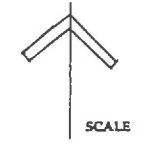
Amendment February 1995  
 PROVIDE GREEN LINK TO PROPOSED BRUSHY CREEK LINEAR PARK

PEDESTRIAN BRIDGE

2M WIDE GRAVEL PATH

START TREE REVEGETATION STRATEGY BY PROGRESSIVELY REMOVING WILLOWS AND REPLACING THEM WITH NATIVE EUCALYPTUS AND ACACIA TREES

LENAH VALLEY ROAD/LADY FRANKLIN MUSEUM PARK ENTRY  
 • A SENSE OF ENTRY  
 • SIGNAGE  
 • 'TOTEMS' (ART WORK)  
 • PLANTING  
 • PARKING



# NEW TOWN RIVULET LINEAR PARK

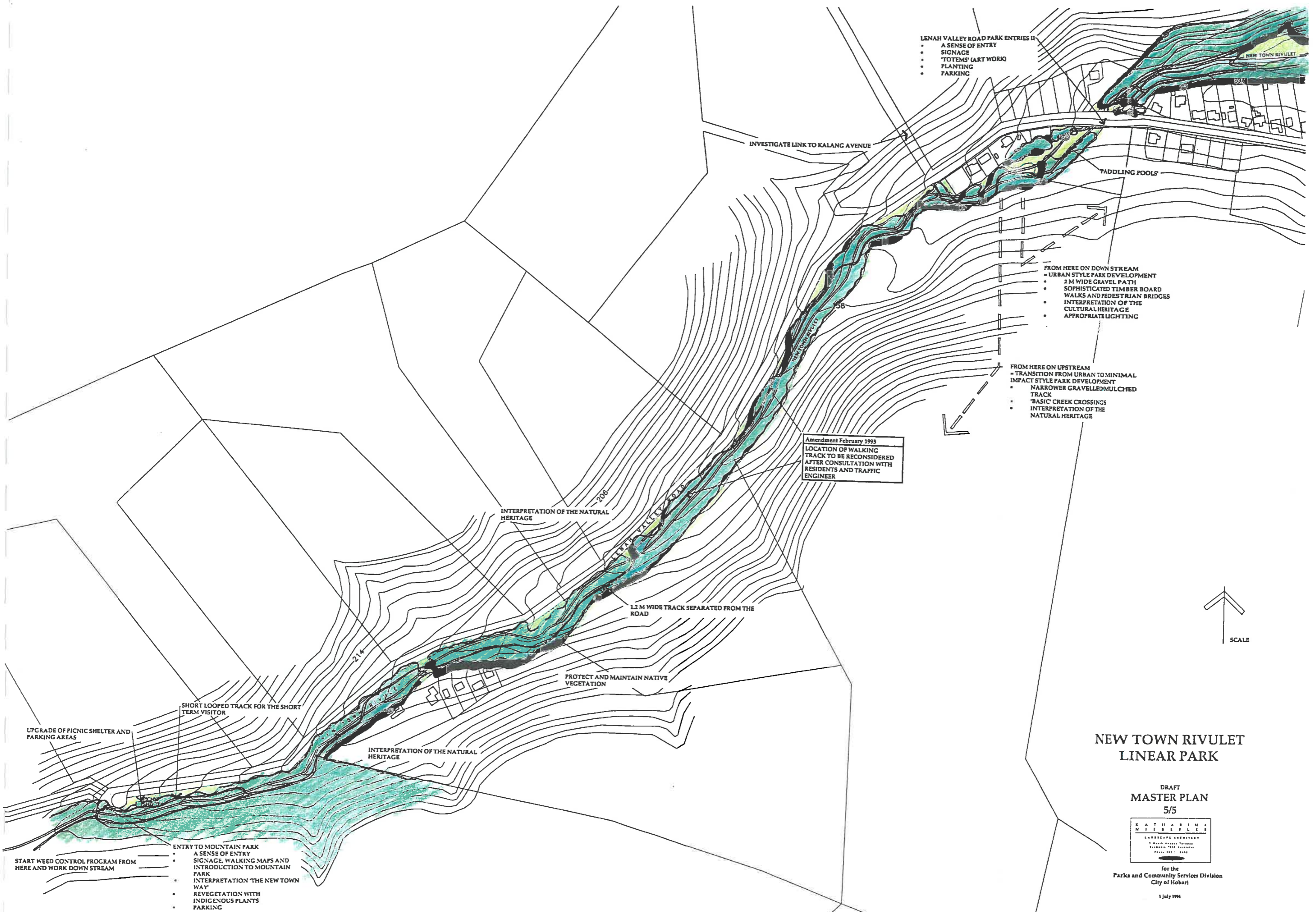
DRAFT  
 MASTER PLAN  
 4/5



for the  
 Parks and Community Services Division  
 City of Hobart

1 July 1994





- LENAH VALLEY ROAD PARK ENTRIES II
- A SENSE OF ENTRY
  - SIGNAGE
  - 'TOTEMS' (ART WORK)
  - PLANTING
  - PARKING

INVESTIGATE LINK TO KALANG AVENUE

PADDLING POOLS

- FROM HERE ON DOWN STREAM
- URBAN STYLE PARK DEVELOPMENT
  - 2 M WIDE GRAVEL PATH
  - SOPHISTICATED TIMBER BOARD WALKS AND PEDESTRIAN BRIDGES
  - INTERPRETATION OF THE CULTURAL HERITAGE
  - APPROPRIATE LIGHTING

- FROM HERE ON UPSTREAM
- TRANSITION FROM URBAN TO MINIMAL IMPACT STYLE PARK DEVELOPMENT
  - NARROWER GRAVELLED/MULCHED TRACK
  - 'BASIC' CREEK CROSSINGS
  - INTERPRETATION OF THE NATURAL HERITAGE

Amendment February 1995  
 LOCATION OF WALKING TRACK TO BE RECONSIDERED AFTER CONSULTATION WITH RESIDENTS AND TRAFFIC ENGINEER

INTERPRETATION OF THE NATURAL HERITAGE

1.2 M WIDE TRACK SEPARATED FROM THE ROAD

PROTECT AND MAINTAIN NATIVE VEGETATION

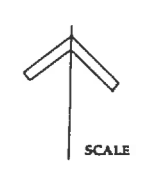
INTERPRETATION OF THE NATURAL HERITAGE

SHORT LOOPED TRACK FOR THE SHORT TERM VISITOR

UPGRADE OF PICNIC SHELTER AND PARKING AREAS

- ENTRY TO MOUNTAIN PARK
- A SENSE OF ENTRY
  - SIGNAGE, WALKING MAPS AND INTRODUCTION TO MOUNTAIN PARK
  - INTERPRETATION 'THE NEW TOWN WAY'
  - REVEGETATION WITH INDIGENOUS PLANTS
  - PARKING

START WEED CONTROL PROGRAM FROM HERE AND WORK DOWN STREAM



# NEW TOWN RIVULET LINEAR PARK

DRAFT  
 MASTER PLAN  
 5/5



for the  
 Parks and Community Services Division  
 City of Hobart

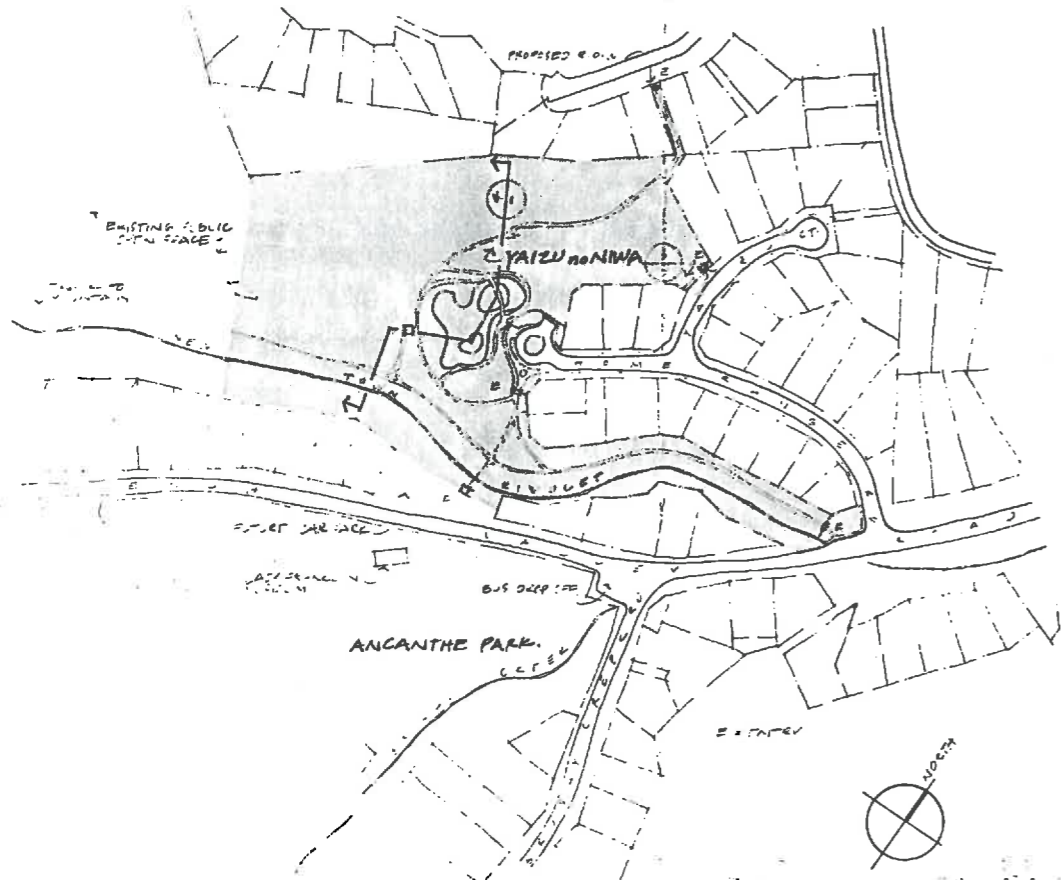
1 July 1994

## Appendix C

### Yaizu no Niwa Landscape Plan

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# 焼津の庭

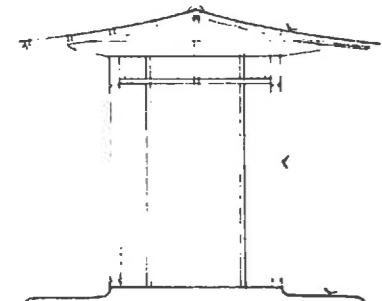


1. LOCALITY PLAN  
SCALE 1:2000

SOURCE PLAN: DEMOGRAPHIC PHOTOS HEIGHT 41.31  
LAND PARS AND SURROUNDINGS

COPY'S COPY & SHEET REF

REFINED VARIETY PLANT



ELEVATION

2. PAVILION  
NOT TO SCALE

3. CONCEPT PLAN  
SCALE 1:500

NOTE:  
PROPOSED PLANTINGS IN JAPANESE  
STYLE USING AUSTRALIAN NATIVE  
PLANTS.  
REVEGETATION USES LOCAL  
AND COMMON FLORA.



PAVILION

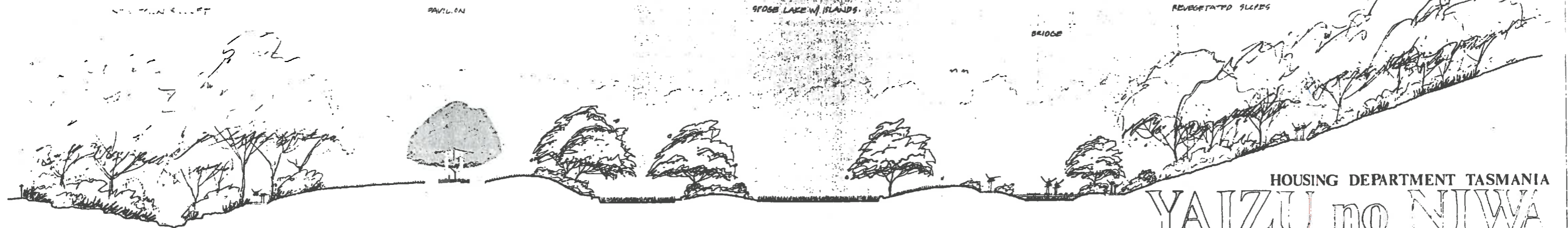
PAVILION

PROPOSED LAKE W/ ISLANDS

REVEGETATED SLOPES

BRIDGE

4. SITE SECTION  
SCALE 1:200



HOUSING DEPARTMENT TASMANIA

## YAIZU no NIWA

JERRY deGRYSE p/1 JERRY deGRYSE & LANDSCAPE ARCHITECT

## **Appendix D**

**Wilmslow Avenue  
Rivulet Park & Sculpture Playground**

TIMBER DECK WITH HIDEAWAY ACCESS TO ENJOY CLOSE-UP VIEWS OF THE CREEK.

STONE-CARVED SEAT WITH WALNUT SLATS NEXT TO PROVIDE A PLACE TO SIT ALONG THE VERY PEACEFUL, PRETTY RIVULET.

GRAVEL PATH TO MEANDER THROUGH TREES TO CONNECT TO UNITS & HOUSES BEYOND.

SIMILARLY PATH TO BRANCH OFF TO CONTINUE FOLLOWING NEW RIVULET TRACE. STONE-EDGE WHERE THE PATHS INTERSECT.

PROVIDE MORE CARVED CELERY-TOP POLES, PAINTED AT THIS INTERSECTION. GENERAL THEME OF CARVED POLES TO BE TROPICAN BIRDS - POSSIBLY THOSE FOUND ALONG TRACE.

STONE-EDGED PATH AS PART OF THE PROPOSED NEW TOWN RIVULET WALK.

THIS PART OF THE WALK EMPHASIZES THE HISTORIC FEEL OF THE PRECINCT. THE PATH OUTLINES THE FOUNDATIONS OF OLD STORES BUILT BY THE ORIGINAL PROPERTY OWNER, CAPTAIN SWANSON IN THE 1840s.

NEW FOOTBRIDGE ACROSS NEW TOWN RIVULET BY HOUST CITY COUNCIL

NEW GRAVEL PATH 1.2M WIDE, WINDING DOWN TO THE RIVULET - 1:14 MIN.

ENRICHMENT FOR LOCAL COMMUNITY TO BE SET INTO BANK BY HOUST CITY COUNCIL. SURROUND WITH TREES & TUFTY SHRUBS.

NEW SIGNAGE FOR MODIFYING TRAFFIC SPEED WHEN ENTERING WILMSLOW AVE - HOUST CITY COUNCIL PROVIDE (3) TROPICAN CARVED CELERY-TOP POLES AS ENTRANCE GATEWAY TO THE STREET.

PLANT ADDITIONAL STREET TREES TO CONTINUE AVENUE PLANTING OF THE STREET.

### LEGEND

- EXISTING TREES & SHRUBS
- PROPOSED TREES & SHRUBS
- EXISTING CONTOURS - .5M INTERVAL
- LOW GRAVEL PATH
- GRASS

### THE RIVULET PARK

Here is the park by New Town Rivulet, a peaceful setting with overhanging willows, a lush grassy verge and a gurgling, ever-changing stream running by.

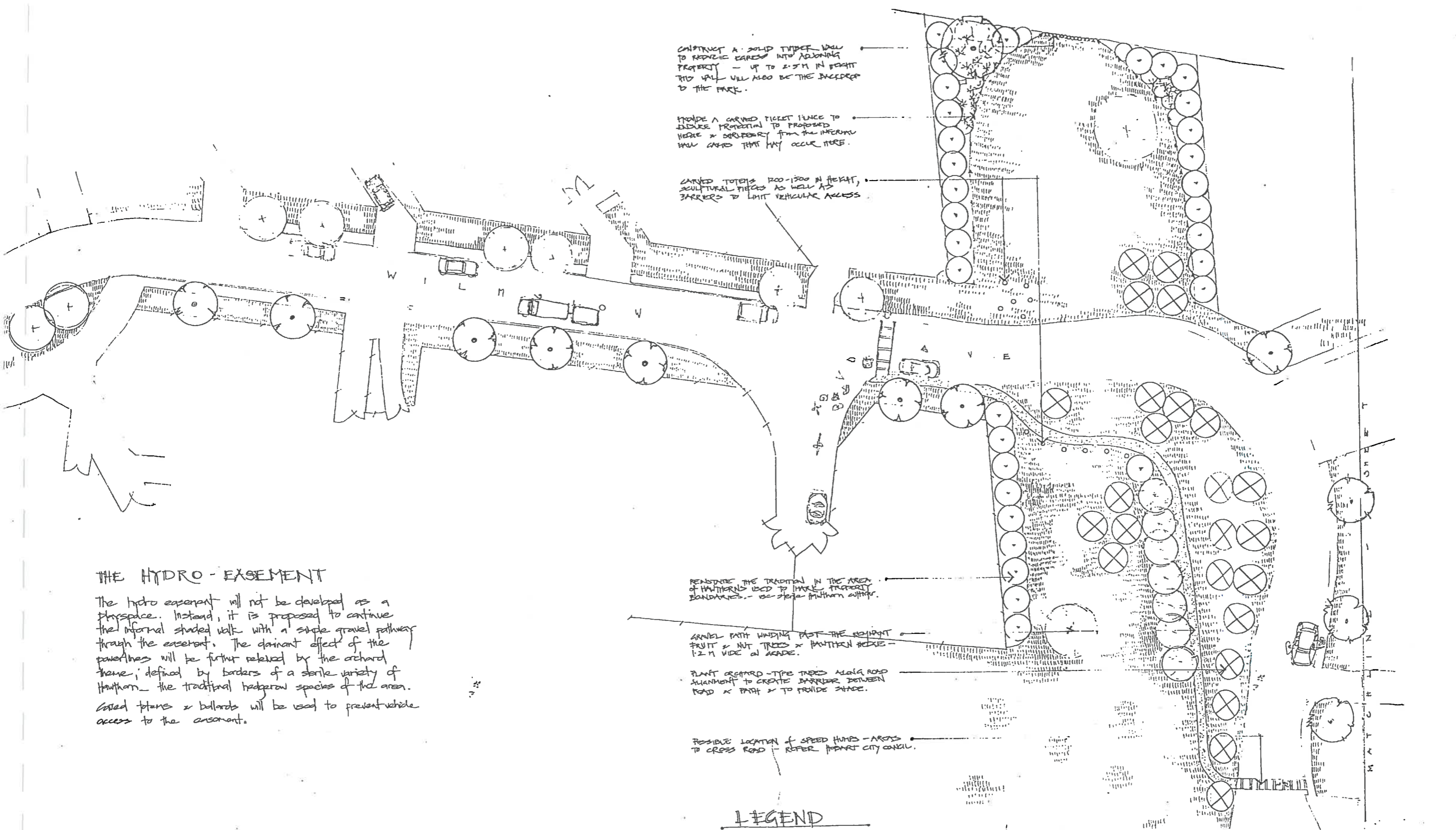
The development plan for the area provides a pathway, barbecue, seating & viewing deck in a pleasant setting.

By taking a sculptural approach to the various elements the area will be unique yet retain the essential character of the area.

# WILMSLOW AVE

## RIVULET PARK & SCULPTURE PLAYGROUND

DRAWING TITLE				REVISIONS
SCHEMATIC PLAN - RIVULET PARK				
SCALE	DATE	DESIGNED	DRAWN	SHEET NO.
1:200	MAY '03	S. TAYLOR, S. SMILL A. HOCHCOCK, K. BUCKLE	SM	1A/3
AN AUSTRALIAN COUNCIL-COMMUNITY ARTS PROJECT - A				COMMUNITY ENVIRONMENTAL ARTS & DESIGN



### THE HYDRO-EASEMENT

The hydro easement will not be developed as a physpace. Instead, it is proposed to continue the informal shaded walk with a simple gravel pathway through the easement. The dominant effect of the powerlines will be further relaxed by the orchard theme, defined by borders of a sterile variety of Hawthorn - the traditional hedgerow species of the area. Carved totems & bollards will be used to prevent vehicle access to the easement.

### LEGEND

- EXISTING TREES
- NEW TREES & SHRUBS
- ORCHARD TREES
- HAWTHORN HEDGES
- GRASS
- GRAVEL PATH
- TOTEMS, BOLLARDS

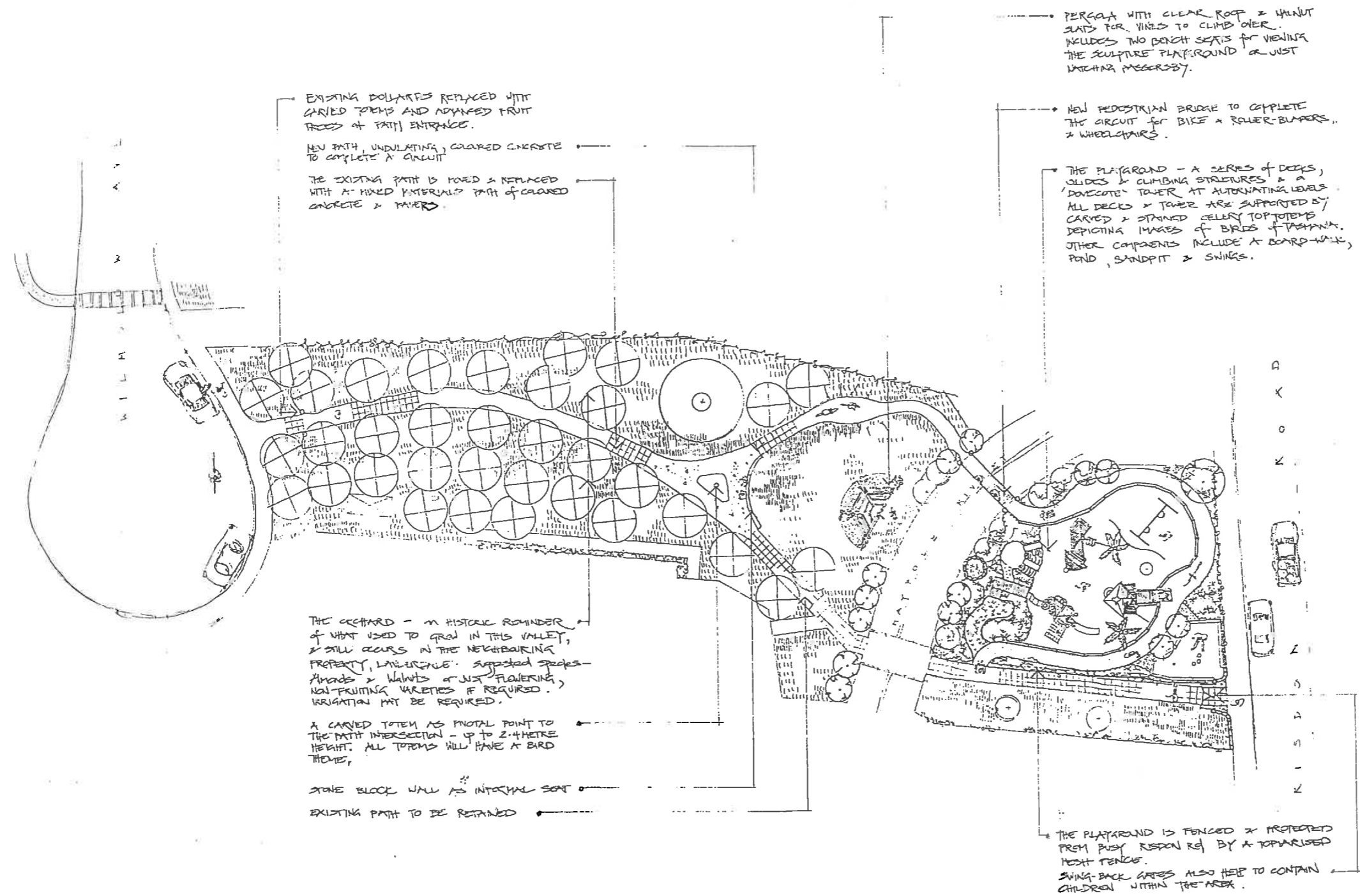
# WILMSLOW AVE

## RIVULET PARK & SCULPTURE PLAYGROUND

DRAWING TITLE SCHEMATIC PLAN - THE HYDRO-EASEMENT				REVISIONS
SCALE 1:200	DATE MAY '03	DESIGNED S. SIMON & M. WALDENE A. WILKINSON	DRAWN S. SIMON & M. WALDENE	SHEET NO. 2A/3

# LEGEND

- Existing TREES & SHRUBS
- Proposed TREES, SHRUBS, CLIMBERS
- NEW ORCHARD
- NEW CONCRETE & PAVED PATHWAY
- EXISTING CONCRETE PAVED PATHWAY
- GRASS
- SYNTHETIC GRASS
- DECKS, TIRES ETC.
- SANDPIT
- TOTEMS, DOLLARDS
- GRAVEL PATHS OVERSURFACING
- MULCH UNDERSURFACE



EXISTING DOLLARDS REPLACED WITH CARVED TOTEMS AND ADVANCED FRUIT TREES AT PATH ENTRANCE.

NEW PATH, UNULATING, COLORED CONCRETE TO COMPLETE A CIRCUIT

THE EXISTING PATH IS MOVED & REPLACED WITH A MIXED MATERIAL PATH OF COLORED CONCRETE & PAVERS.

PERGOLA WITH CLEAR ROOF & WALNUT SLATS FOR VINES TO CLIMB OVER. INCLUDES TWO BENCH SEATS FOR VIEWING THE SCULPTURE PLAYGROUND OR JUST WATCHING PASSESERSBY.

NEW PEDESTRIAN BRIDGE TO COMPLETE THE CIRCUIT FOR BIKE & POWER-BLANKERS, & WHEELCHAIRS.

THE PLAYGROUND - A SERIES OF DECKS, SLIDES & CLIMBING STRUCTURES & A 'DOVECOTE' TOWER AT ALTERNATING LEVELS. ALL DECKS & TOWER ARE SUPPORTED BY CARVED & STAINED CELERY TOTOTEMS DEPICTING IMAGES OF BIRDS & TASMANIA. OTHER COMPONENTS INCLUDE A BOARD-WALK, POND, SANDPIT & SWINGS.

THE ORCHARD - A HISTORIC REMINDER OF WHAT USED TO GROW IN THIS VALLEY, & STILL OCCURS IN THE NEIGHBOURING PROPERTY, LAKEVILLE. SUGGESTED SPECIES - AVOCADO & WALNUTS (JUST FLOWERING), NON-FRUITING VARIETIES IF REQUIRED. IRRIGATION MAY BE REQUIRED.

A CARVED TOTEM AS FOCAL POINT TO THE PATH INTERSECTION - UP TO 2.4 METRE HEIGHT. ALL TOTEMS WILL HAVE A BIRD THEME.

STONE BLOCK WALL AS INTERNAL SEAT

EXISTING PATH TO BE RETAINED

THE PLAYGROUND IS FENCED & PROTECTED FROM PUSH BIKES BY A TOPHATISED POST FENCE. SWING-BACK GRASS ALSO HELPS TO CONTAIN CHILDREN WITHIN THE AREA.

## THE ORCHARD & SCULPTURE PLAYGROUND

The main pedestrian thoroughfare to Risda Rd will pass through an orchard & sculpture playground. The community decided this area was the most appropriate for a playground.

The playground provides for a wide variety of experiences, including play activities on decks & slides, 'tote' structures, a sandpit & drill tower as well as more passive possibilities; sitting in the quiet areas contemplating the mosaic pond, enjoying the totems with their Tasmanian bird themes, perhaps simply walking by on the undulating pathway or watching the activity from a distance - a parapet-like structure is proposed for the other side of Murrupde Rivulet from where the playground may be viewed.

An extension to the new path, predominantly to complete an undulating bike track circuit is also proposed as the children of the community felt this to be one of the most important requirements if any development were to occur.

# WILMSLOW AVE

## RIVULET PARK & SCULPTURE PLAYGROUND

DRAWING TITLE				REVISIONS	
SYMBOLIC PLAN - ORCHARD & SCULPTURE PLAYGROUND					
SCALE	DATE	DESIGNED	DRAWN	SHEET NO.	
1:200	MAY 93	Audrey Hutchinson, Stuart Thomas, Rhonda Hobbins, Sue Small	S.S. A.H.	3A/3	
- AN AUSTRALIAN COUNCIL-COMMUNITY ARTS PROJECT - GRANT					





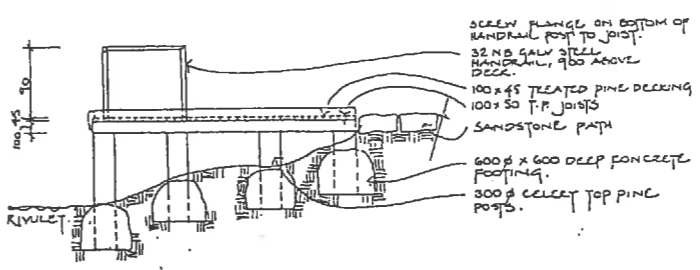
GRAVEL PATH, 1000 WIDE, EDGED WITH SANDSTONE AT JUNCTIONS  
LINE OF EXISTING HAWTHORNE HEDGE  
3 TOTEMS, 2x1200 HIGH, 1x 2400 HIGH

TOTEMS AT ENTRANCE TO WILMSLOW AVE. G. ON TRAFFIC ISLAND.  
SANDSTONE EDGED GRAVEL PATH 3000 WIDE.  
SEE DETAIL Z  
SANDSTONE AND TIMBER DEATS. SEE DETAIL 1  
DECK AROUND EXISTING TREE  
GRAVEL PATH TO PROPOSED H.C.C. FOOTBALL FIELD  
BARBEQUE BY H.C.C.

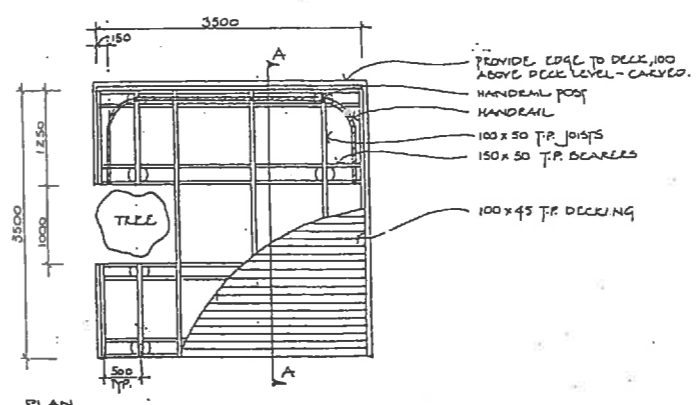
FUTURE POSITION OF B.B.P. BY H.C.C.

PLANTING KEY

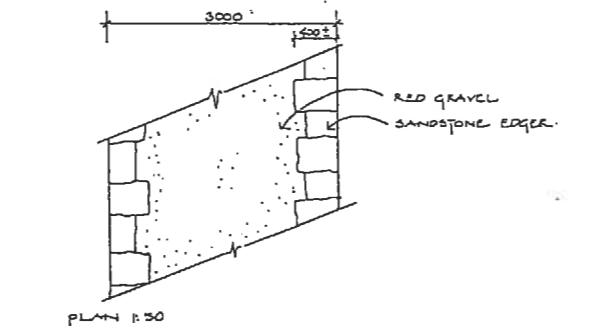
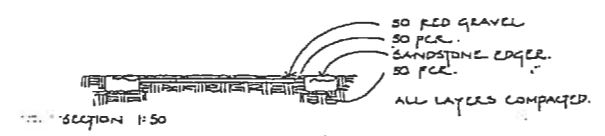
-  EXISTING TREES
-  PROPOSED SHRUBS



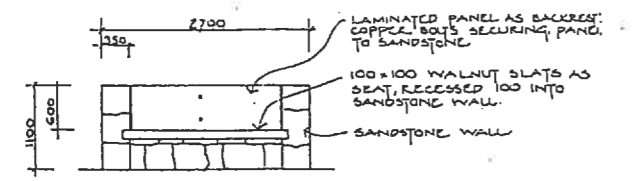
ELEVATION/SECTION A-A  
1:50



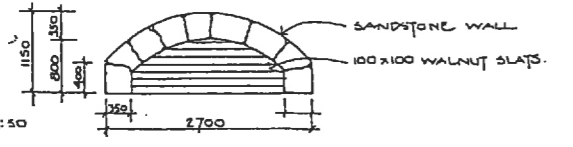
PLAN 1:50  
3 DECK AROUND TREE



2 SANDSTONE/GRAVEL PATH



ELEVATION 1:50



1 SEAT

# WILMSLOW AVE

## RIVULET PARK & SCULPTURE PLAYGROUND

DRAWING TITLE				REVISIONS	
RIVULET PARK AND DETAILS					
SCALE	DATE	DESIGNED	DRAWN	SHEET NO.	
AS SHOWN	MAY 1993	RJONDDA HALDANE AUDREY HUTCHISON STUART THORNE SUSAN SMALL	CE	1	
AN AUSTRALIAN COUNCIL-COMMUNITY ARTS PROJECT					



CONSTRUCT A SOLID TIMBER WALL TO REDUCE EXCESS INTO ADJOINING PROPERTY - UP TO 2.5M IN HEIGHT THIS WALL WILL ALSO BE THE BACKDROP TO THE PARK.

INSTALL A CARVED TICKET FENCE TO ENSURE PROTECTION TO PROPOSED HERB & ORCHARD FROM THE INTERFERING WALL CARVE THAT MAY OCCUR HERE.

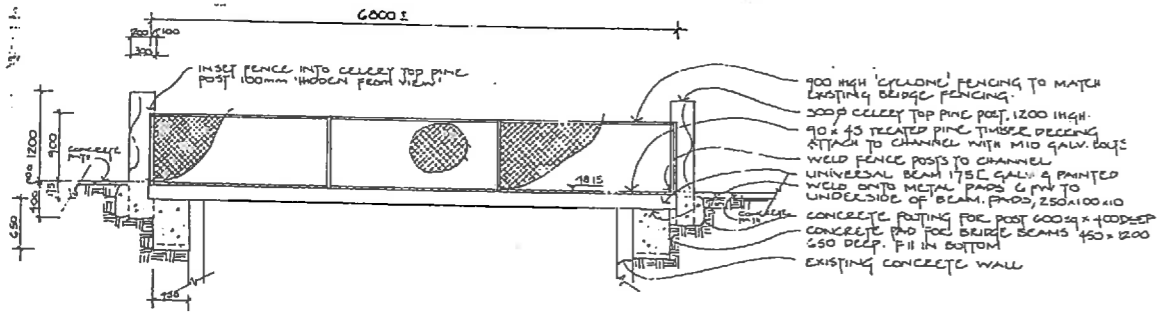
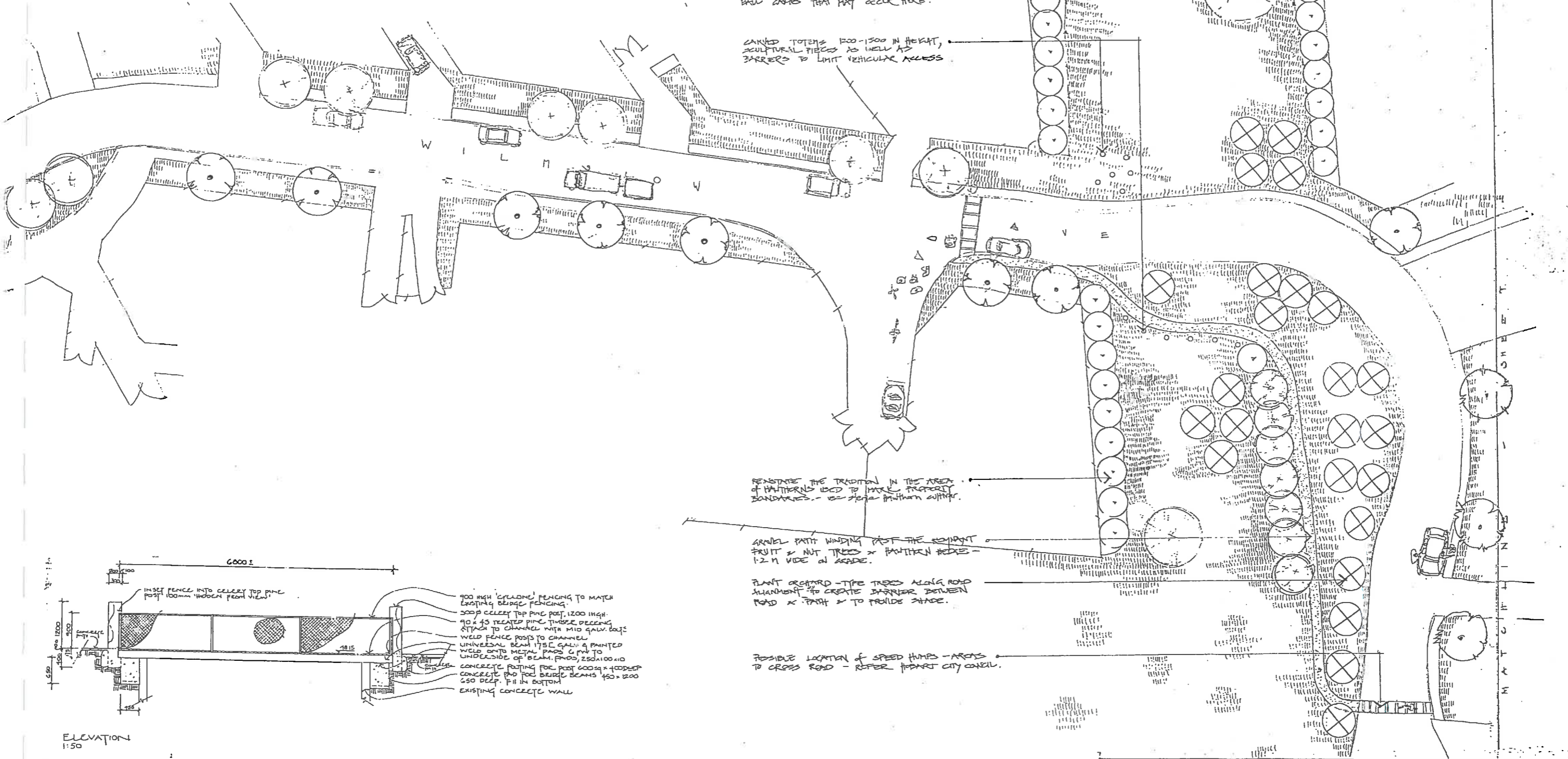
CARVED TOTEMS 100-1500 IN HEIGHT, SCULPTURAL PIECES AS WELL AS BARRERS TO LIMIT VEHICULAR ACCESS

REINSTATE THE TRADITION IN THE AREA OF HATHORN'S USED TO MARK PROPERTY BOUNDARIES - 100 YEAR HATHORN CULTURE.

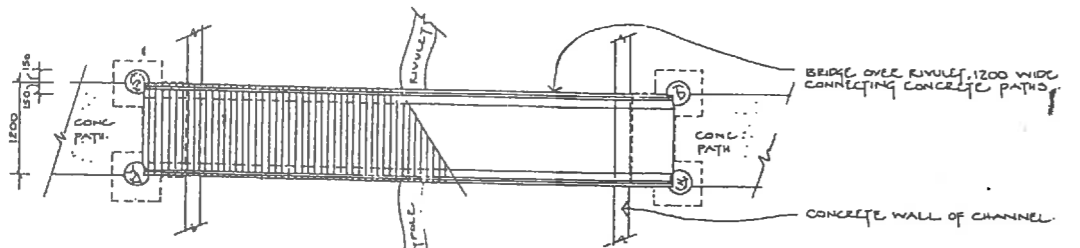
GRAVEL PATH WINDING PAST THE REMOVED FRUIT & NUT TREES & HATHORN TREES - 1.2 M WIDE ON GRADE.

PLANT ORCHARD - TYPE TREES ALONG ROAD ALIGNMENT TO CREATE BARRIER BETWEEN ROAD & PATH & TO PROVIDE SHADE.

POSSIBLE LOCATION OF SPEED BUMPS - AGRICULTURE TO CROSS ROAD - REFER HODART CITY COUNCIL.



ELEVATION 1:50

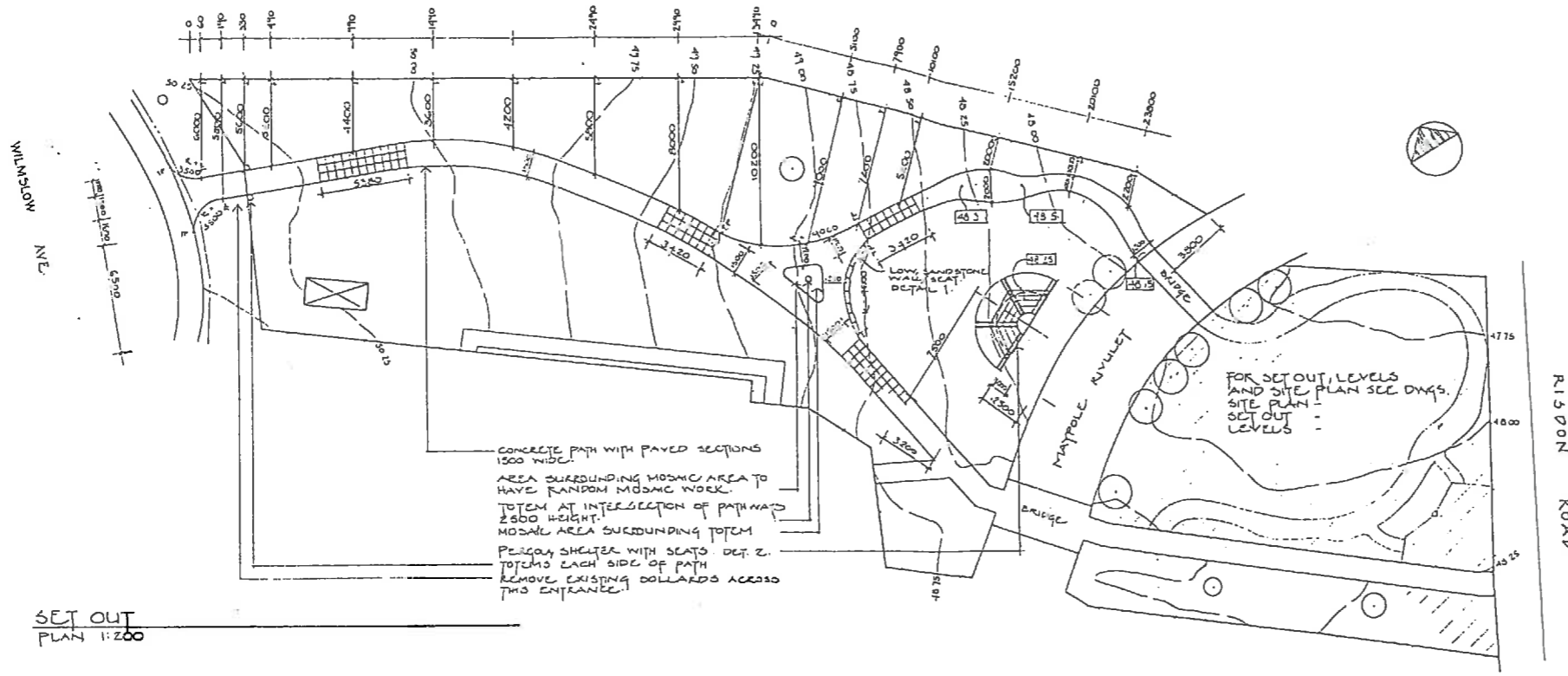


PLAN OF PEDESTRIAN BRIDGE ACROSS HATHORN RIVULET 1:20

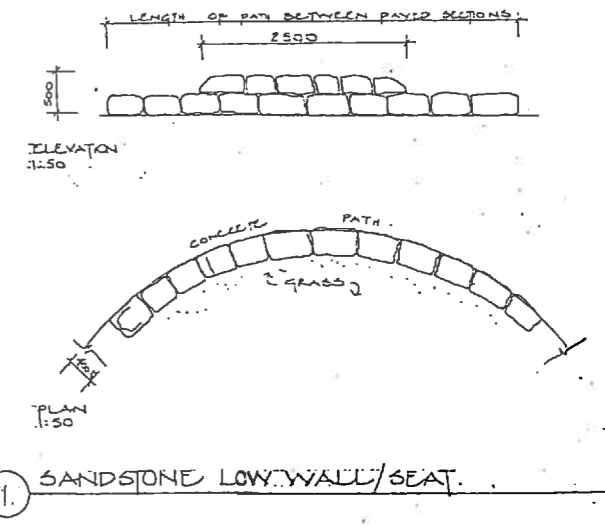
# WILMSLOW AVE

## RIVULET PARK & SCULPTURE PLAYGROUND

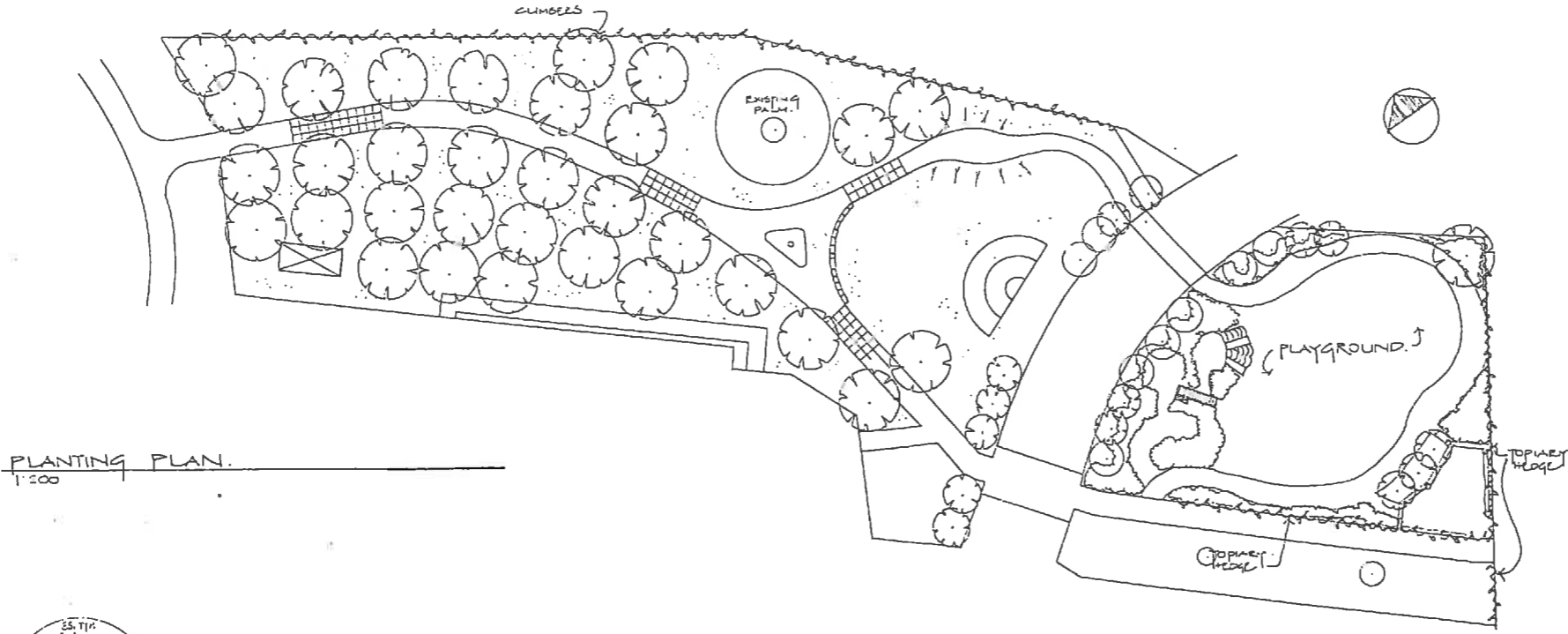
DRAWING TITLE		REVISIONS		
HYDRO EASEMENT				
SCALE	DATE	DESIGNED	DRAWN	SHEET NO.
1:200	MAY '93	S. SHAW S. THOMAS R. WILMSLOW A. WILMSLOW	S. SHAW	2/8



SET OUT  
PLAN 1:200

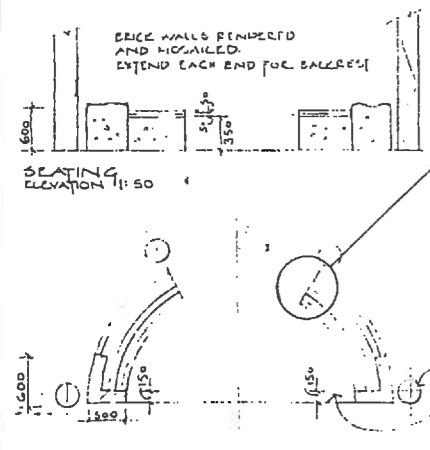


1. SANDSTONE LOW WALL/SEAT.

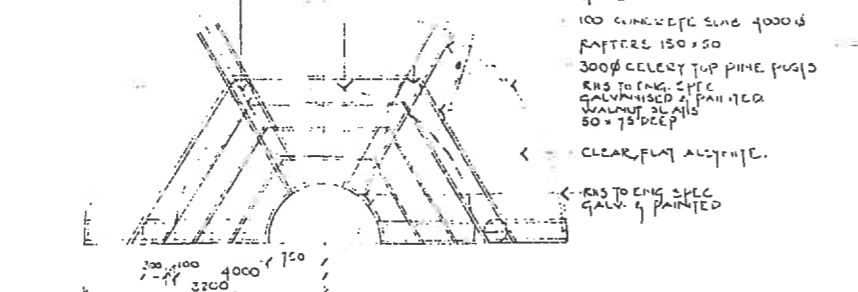


PLANTING PLAN  
1:500

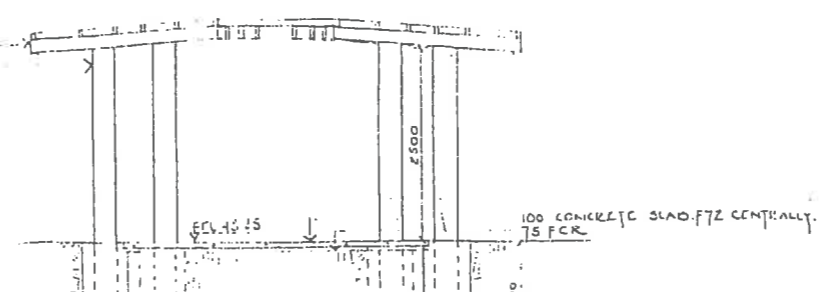
- KEY.**
- PROPOSED TREES
  - PROPOSED SHRUBS
  - CUMBOLES OR TOPINERY HEDGE.
  - PROPOSED FINISHED LEVEL ON PATH AND PERGOLA SURF.
  - EXISTING TREES.



SEATING  
PLAN 1:50  
PERGOLA AND SEATING



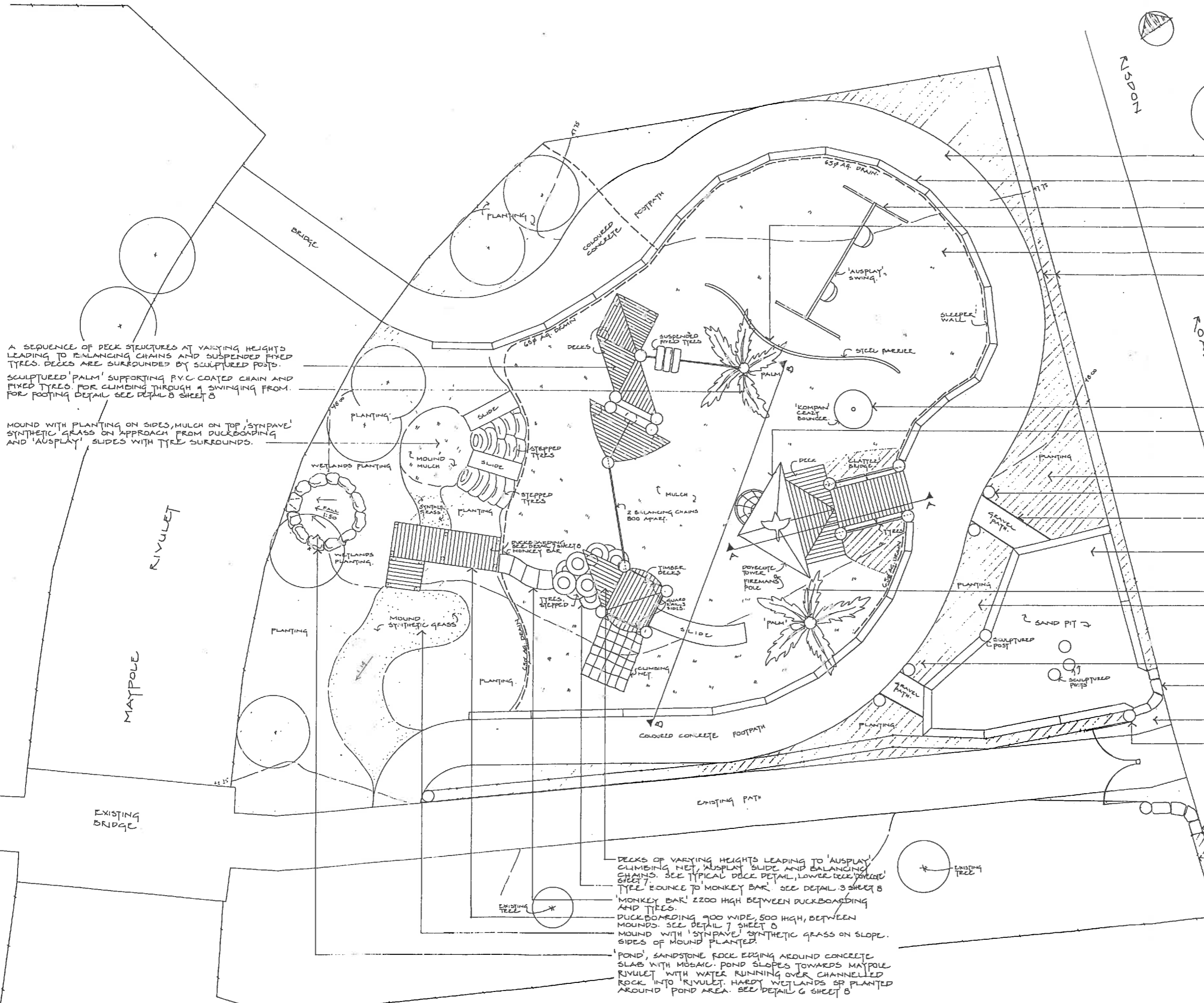
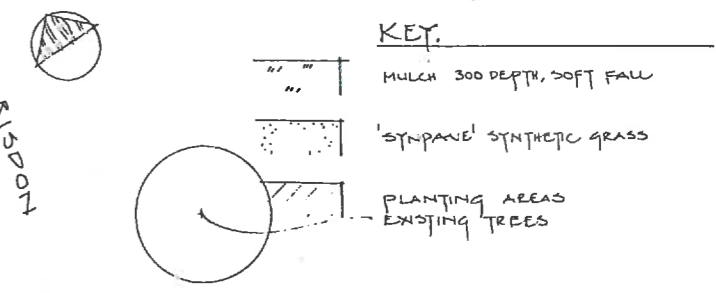
PERGOLA  
PLAN 1:50



PERGOLA  
ELEVATION 1:50

# WILMSLOW AVE RIVULET PARK & SCULPTURE PLAYGROUND

DRAWING TITLE		PLAYGROUND & DETAILS		REVISIONS
SCALE	DATE	DESIGNED	DRAWN	SHEET NO.
AS SHOWN	MAY 1993	REHONDA WALDANE MURIEL WILMSLOW STUART THORNTON OSMAN SMITH	CE	3



A SEQUENCE OF DECK STRUCTURES AT VARYING HEIGHTS LEADING TO BALANCING CHAINS AND SUSPENDED FIXED TIRES. DECKS ARE SURROUNDED BY SCULPTURED POSTS. SCULPTURED 'PALM' SUPPORTING P.V.C COATED CHAIN AND FIXED TIRES FOR CLIMBING THROUGH & SWINGING FROM. FOR FOOTING DETAIL SEE DETAIL 8 SHEET 8

MOUND WITH PLANTING ON SIDES, MULCH ON TOP 'SYNPAVE' SYNTHETIC GRASS ON APPROACH FROM DUCKBOARDING AND 'AUSPLAY' SLIDES WITH TREE SURROUNDS.

CONCRETE PATH, UNDULATING AND VARYING IN WIDTH SEE SET OUT PLAN, SHEET 6 AND LEVELS PLAN SHEETS AND DETAIL 1 SHEET 8.  
 SLEEPER WALL AS MULCH CONTAINER FOR PLAY AREA, SEE DETAIL 5 SHEET 8  
 'AUSPLAY' STANDARD SWING.  
 GALVANISED TUBULAR STEEL BARRIER BETWEEN DECKS AND SWING AREA. AVERAGE HEIGHT 900  
 MULCH, 300 DEPTH FOR SOFT FALL.  
 'ARC JACARANDA' FENCE (2) 1200 HIGH, RUNNING PARALLEL, 300 APART FOR TOPIARY SUPPORT.

'KOMPAN' CRAZY BOUNCER.  
 'DOVECOTE' TOWER WITH FIREMANS POLE ENTRANCE/EXIT, AND HOOPED CLIMBING NET ENTRANCE/EXIT. LOWER DECK HAS ACCESS TO PERIMETER PATH VIA 1000 WIDE CLATTER BRIDGE WITH SCULPTURED POSTS AND P.V.C COATED CHAIN HAND RAILS.  
 PLANTING  
 POSTS ADJACENT TO START OF GRAVEL PATHS.

GRAVEL PATH TO SANDPIT 700 WIDE WITH TIMBER EDGE. SEE DETAIL 2 SHEET 8  
 SANDPIT, 600 DEEP WITH SLEEPER WALL AS CONTAINER EDGE. 1 SCULPTURED POST IN SLEEPER WALL, 3 SCULPTURED POSTS IN CENTRE OF SANDPIT. SEE SANDPIT DETAIL 4 SHEET 8  
 SCULPTURAL PALM 3000 HIGH. SEE DETAIL 8 SHEET 8  
 PLANTING

GRAVEL PATH, 700 WIDE WITH TIMBER EDGE SEE DETAIL 2 SHEET 8  
 SANDSTONE WALL 1200 HIGH, ADJACENT TO 'ARC' FENCE.  
 COLOURED CONCRETE WITH MOSAIC EACH SIDE OF EXISTING PATH FOR WIDER ACCESS TO PARK.  
 SCULPTURED POSTS AND GATES WITH REMOVABLE BOLLARD TO ALLOW 2200 CLEARANCE FOR PARK MACHINERY.

REMOVE EXISTING BOLLARDS ALONG FENCELINE  
 'ARC JACARANDA' FENCE (2) 1200 HIGH, RUNNING PARALLEL, 300 APART FOR TOPIARY SUPPORT.

NOTE: FOR SECTIONS A-A AND B-B SEE DWG. SHEET 7.



DECKS OF VARYING HEIGHTS LEADING TO 'AUSPLAY' CLIMBING NET, 'AUSPLAY' SLIDE AND BALANCING CHAINS. SEE TYPICAL DECK DETAIL, LOWER DECK 'DOVECOTE' SHEET 7.  
 TREE BOUNCE TO 'MONKEY BAR' SEE DETAIL 3 SHEET 8  
 'MONKEY BAR' 2200 HIGH BETWEEN DUCKBOARDING AND TIRES.  
 DUCKBOARDING 900 WIDE, 500 HIGH, BETWEEN MOUNDS. SEE DETAIL 7 SHEET 8  
 MOUND WITH 'SYNPAVE' SYNTHETIC GRASS ON SLOPE. SIDES OF MOUND PLANTED.  
 'POND', SANDSTONE ROCK EDGING AROUND CONCRETE SLAB WITH MOSAIC. POND SLOPES TOWARDS MAYPOLE RIVULET WITH WATER RUNNING OVER CHANNELLED ROCK INTO RIVULET. HARDY WETLANDS OF PLANTED AROUND POND AREA. SEE DETAIL 6 SHEET 8

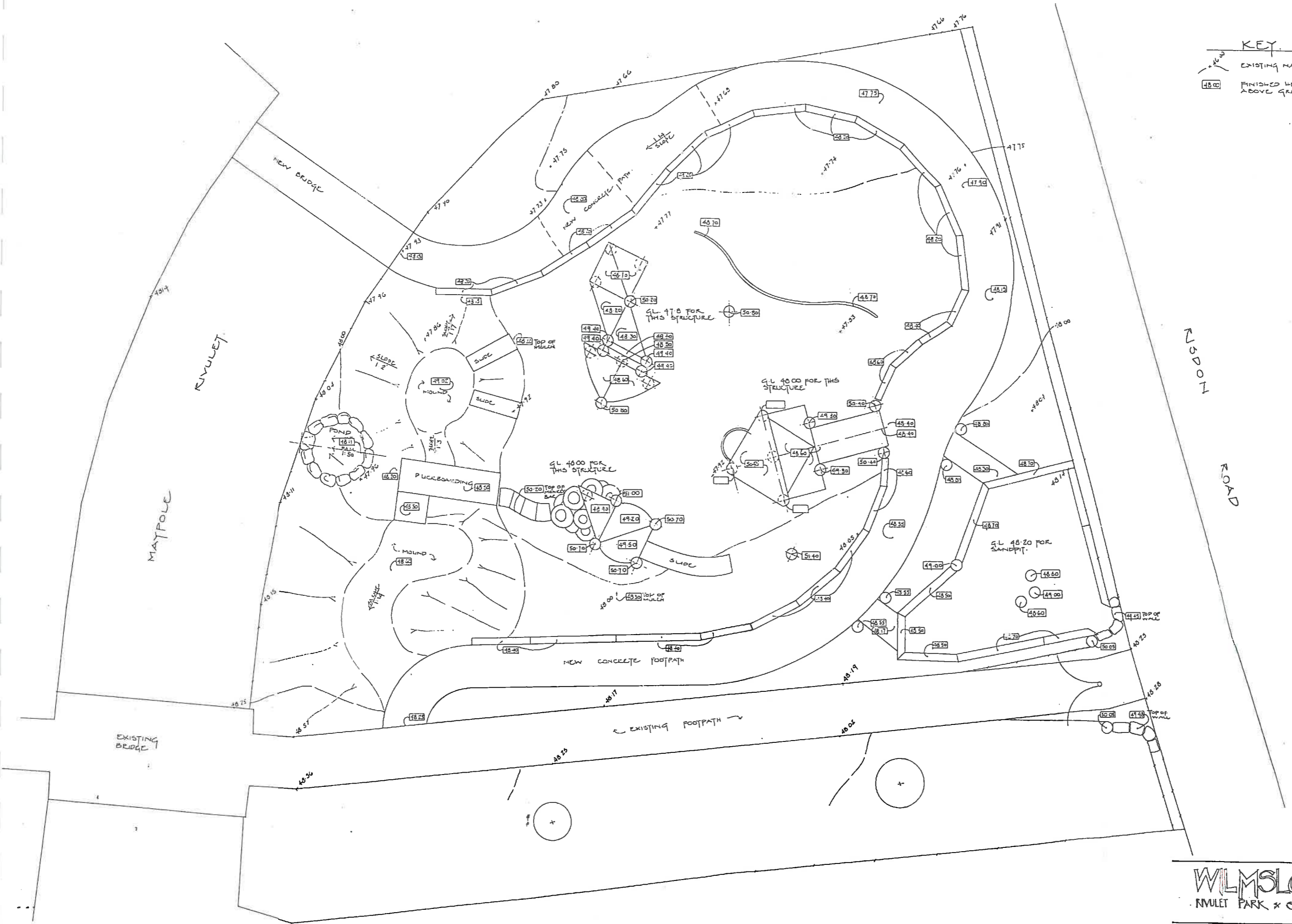
# WILMSLOW AVE

## RIVULET PARK & SCULPTURE PLAYGROUND

DRAWING TITLE	PLAYGROUND SITE PLAN	REVISIONS
SCALE	DATE	DESIGNER
		RHONDA HALDANE
		DRAWN
		SHEET NO.

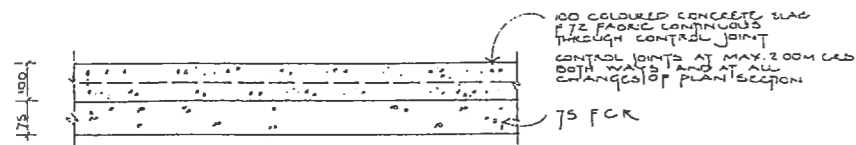
KEY

-  EXISTING NATURAL SURFACE LEVEL
-  FINISHED HEIGHT OF PATH AND STRUCTURES ABOVE GROUND LEVEL.

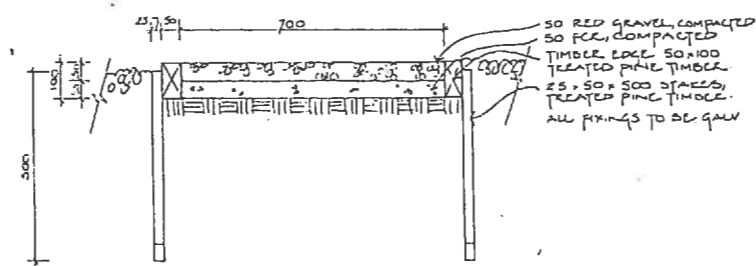


**WILMSLOW AVE**  
RIVULET PARK & SCULPTURE PLAYGROUND

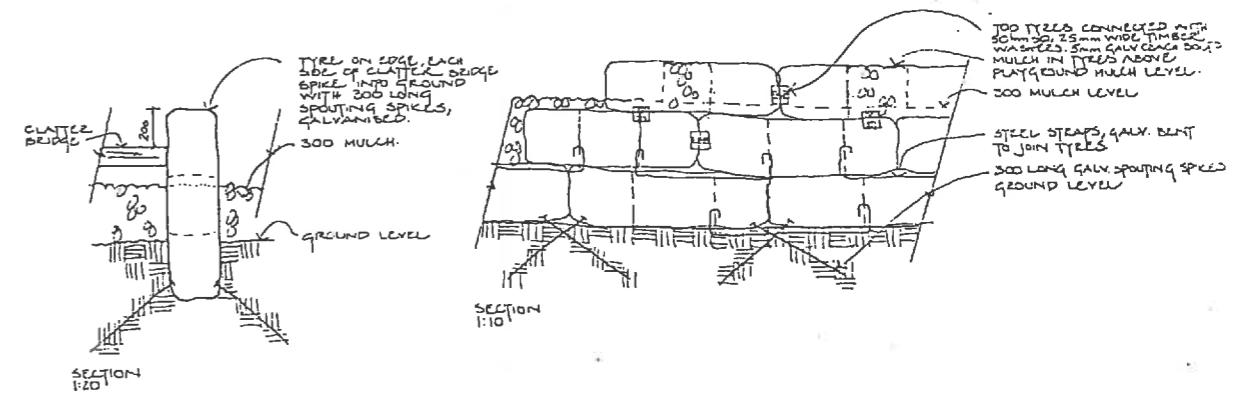
DRAWING TITLE		REVISIONS	
PLAYGROUND LEVELS			
SCALE	DATE	DESIGNER	DRAWN
1:50	MAY 1993	ROBINSON, WILMSLOW, STURGES, JORDAN, BROWN	5



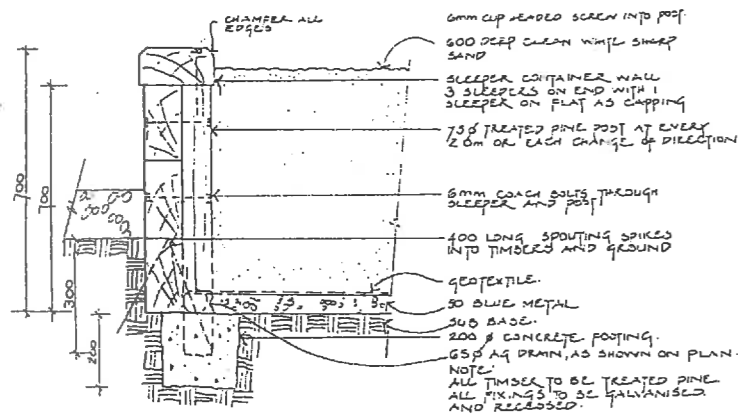
1 CONCRETE PATH  
SECTION 1:10



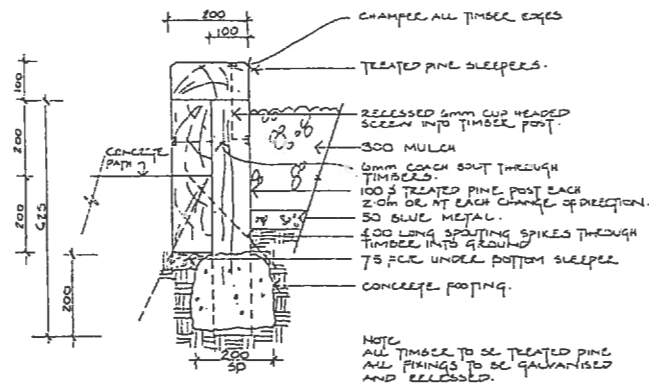
2 GRAVEL PATH AND TIMBER EDGER  
SECTION 1:10



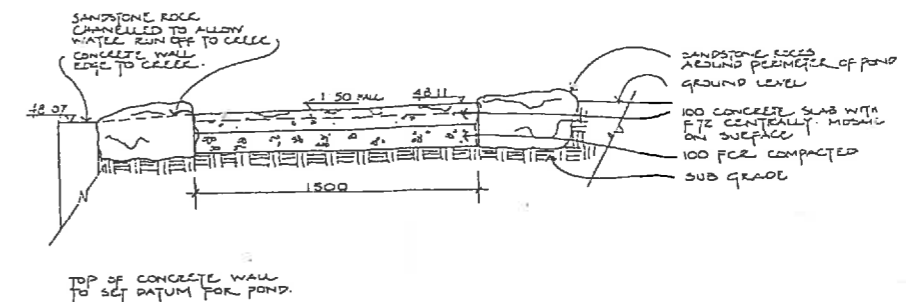
3 TYRE ON EDGE AND TYRE ON FLAT DETAIL  
SECTION 1:10



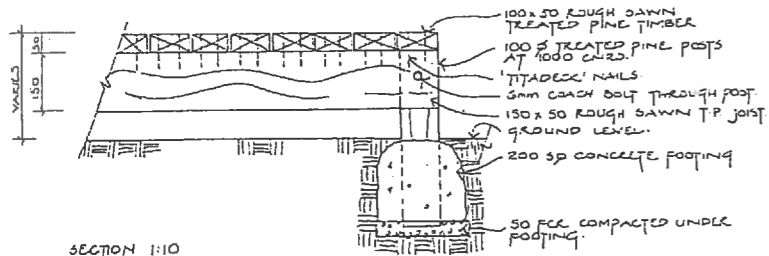
4 SLEEPER SANDPIT  
SECTION 1:10



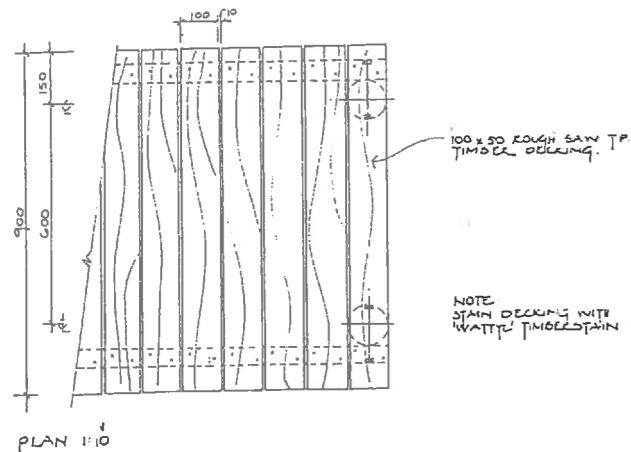
5 SLEEPER WALL  
SECTION 1:10



6 POND  
SECTION 1:20

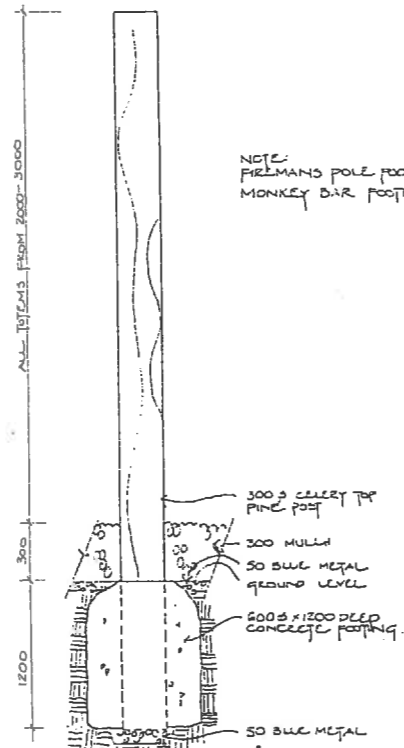


SECTION 1:10

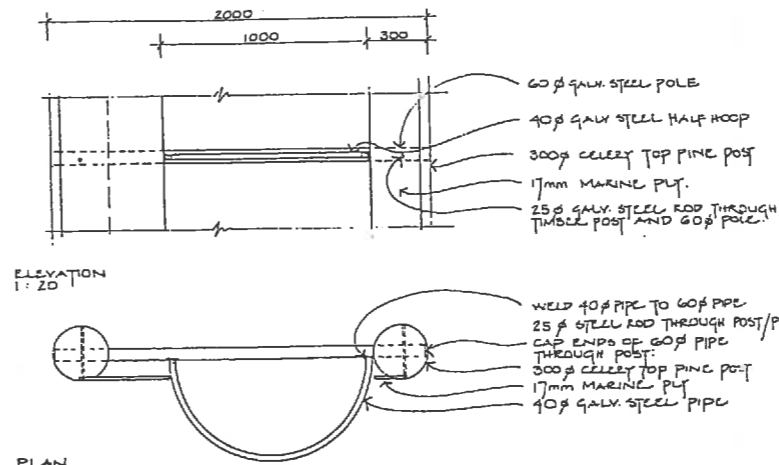


PLAN 1:10

7 DUCKBOARDING



8 TOTEM POLE FOOTING  
SECTION 1:20



9 STEEL PIPE BARRIER & CHAIN NET SUPPORT

WILMSLOW AVE  
MULET PARK & SCULPTURE PLAYGROUND

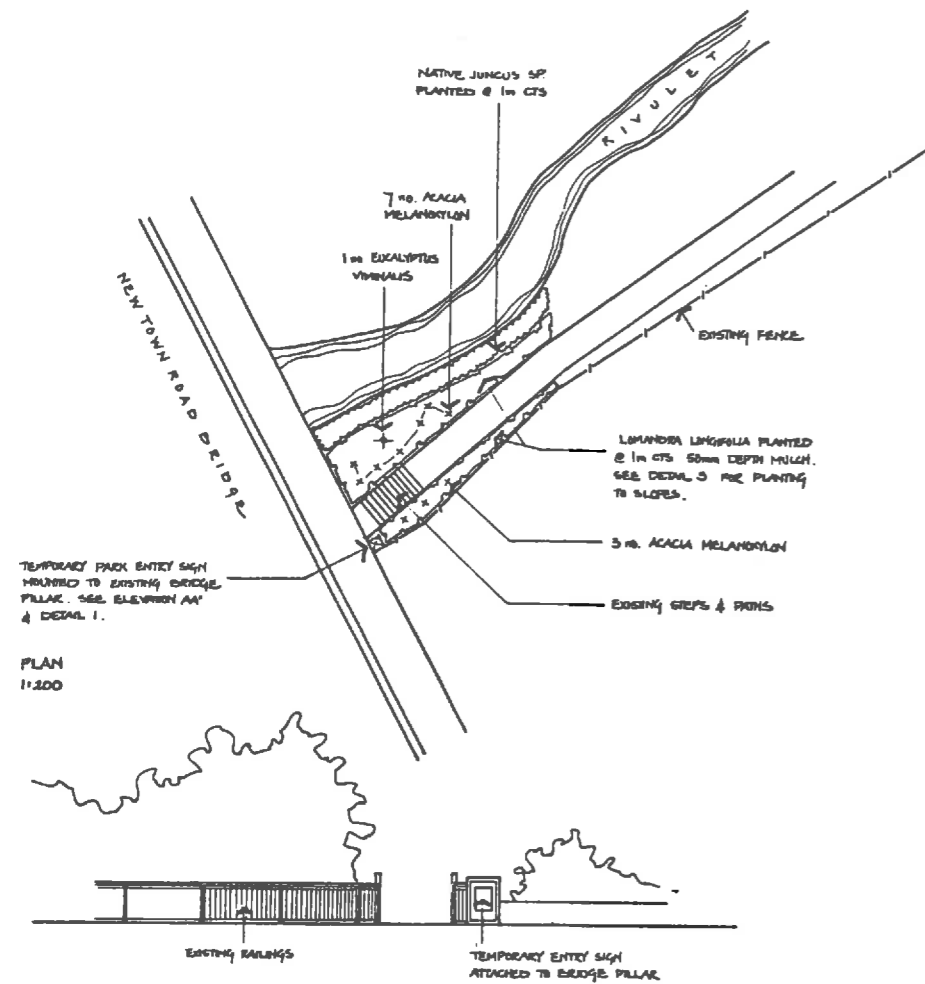
DRAWING TITLE DETAILS				REVISIONS
SCALE AS SHOWN	DATE MAY 1993	DESIGNED RHONDA HALDANE AUDREY HUTCHISON STUART THORNE SUSAN SMALL	DRAWN CE	SHEET NO. 8

## **Appendix E**

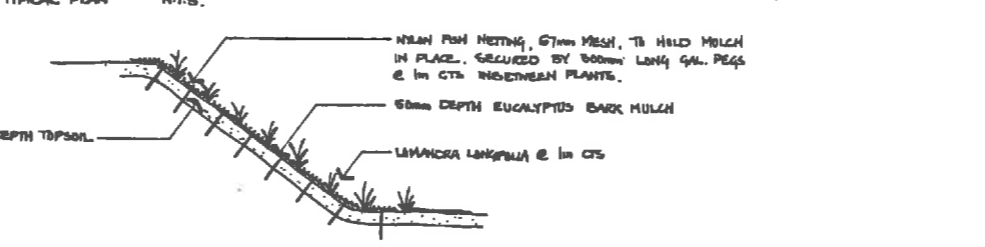
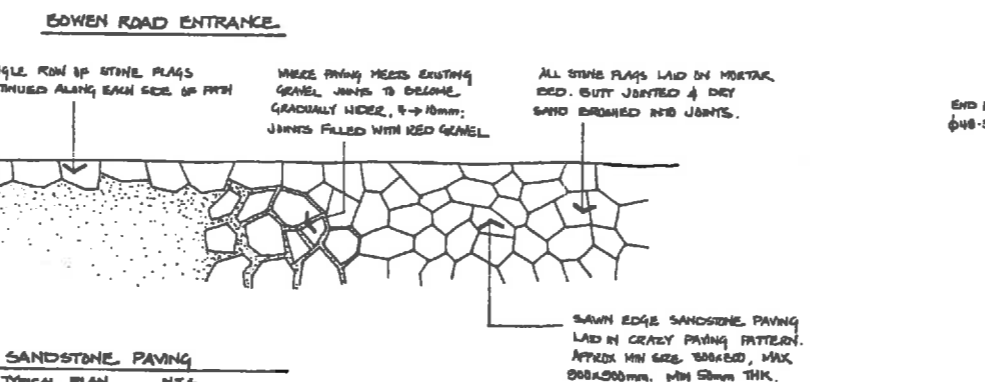
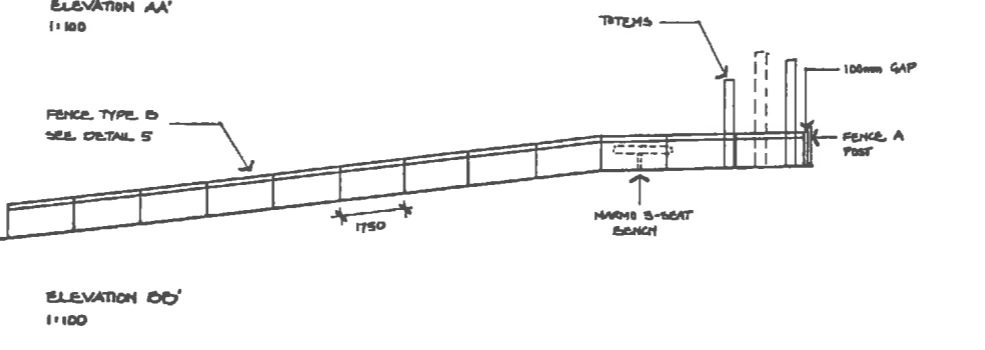
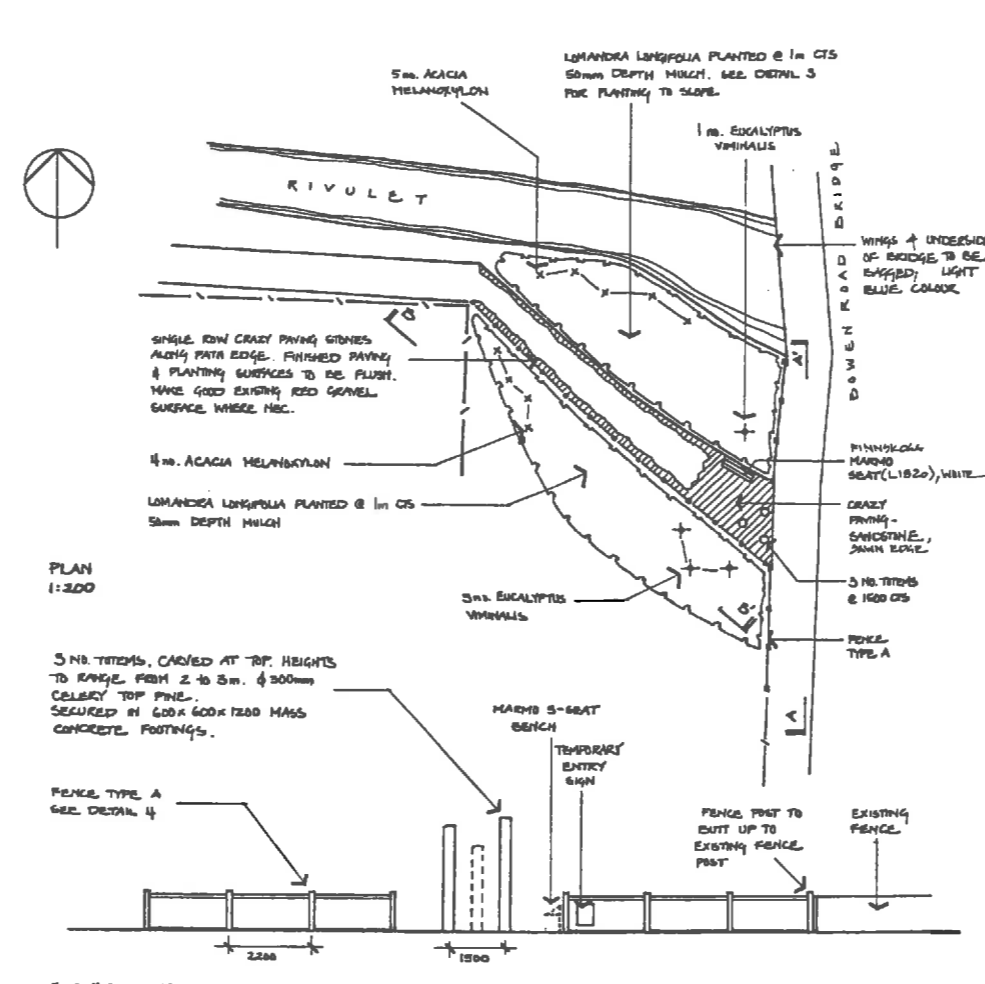
**Landscape Design & Documentation**

**Park Entries**

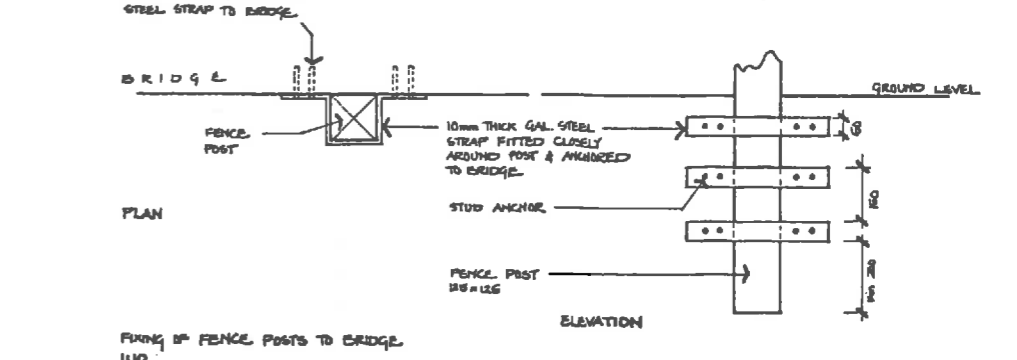
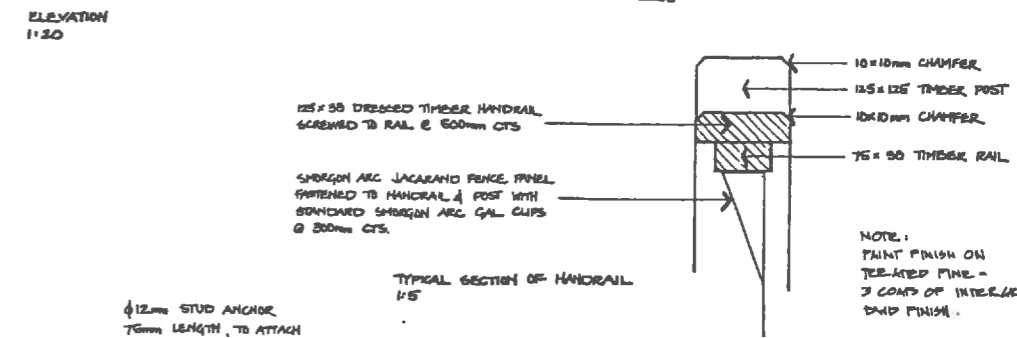
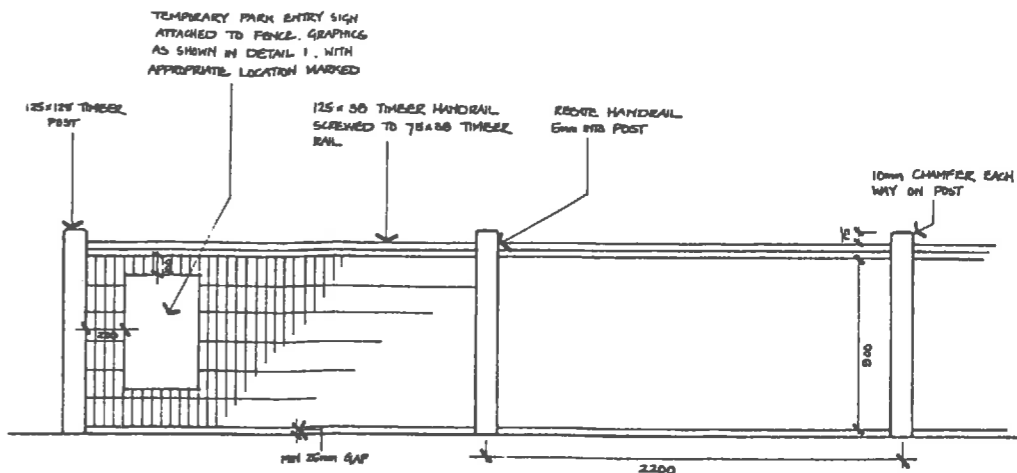
**Tower/Bowen Road and New Town Road**



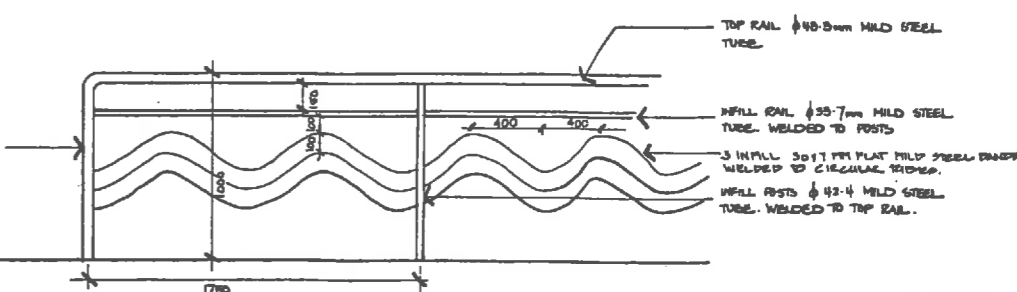
PLAN 1:200  
NEW TOWN ROAD ENTRANCE



2 SANDSTONE PAVING TYPICAL PLAN N.T.S.  
3 PLANTING ON SLOPES TYPICAL SECTION 1:100

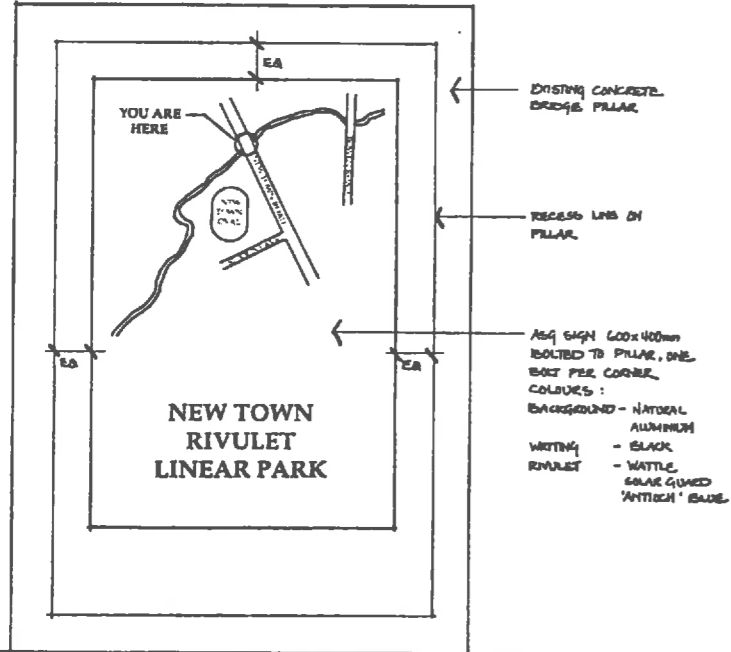


4 FENCE TYPE A TYPICAL DETAILS



5 FENCE TYPE B TYPICAL ELEVATION 1:20

NOTES  
1. It is the contractors responsibility to check the location of services prior to the commencement of work.  
2. It is the responsibility of the contractor to ensure adequate drainage of all planted areas, including tree pits.  
3. All junctions of paving, mulched areas and existing levels are to be flush.  
4. All footings to be founded on stable subgrade.  
5. All timber to be treated Radata Pine. All fixings to be hot dip galvanised steel.  
6. All fixings to be hot dip galvanised steel.  
All Dimensions in mm  
Do not scale  
Verify all dimensions on site before commencement of work  
Any discrepancies shall be referred to the Landscape Architect



1 TEMPORARY ENTRANCE SIGN TYPICAL ELEVATION 1:5

NEW TOWN RIVULET LINEAR PARK  
NEW TOWN ROAD & BOWEN ROAD ENTRIES




For the  
Parks & Community Services Division  
City of Hobart  
3rd April 1995

## **Appendix F**

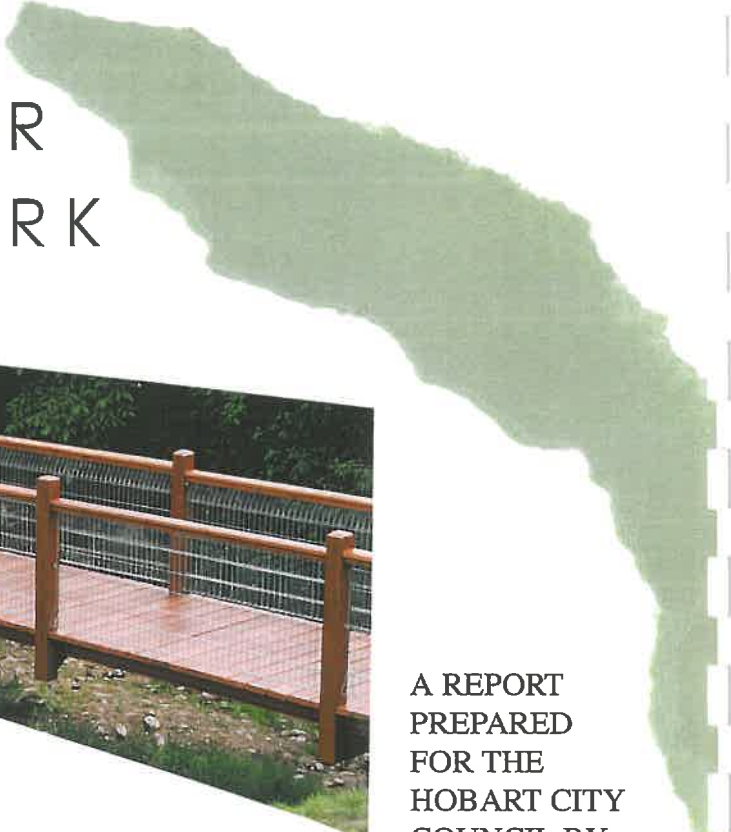
**Summary of Issues discussed at the public meeting.**







THE  
NEW TOWN  
RIVULET  
LINEAR  
PARK



A REPORT  
PREPARED  
FOR THE  
HOBART CITY  
COUNCIL BY:  
KATHARINA  
NIEBERLER,  
LANDSCAPE  
ARCHITECT.  
1 APRIL 1995.

